

SYLMAR

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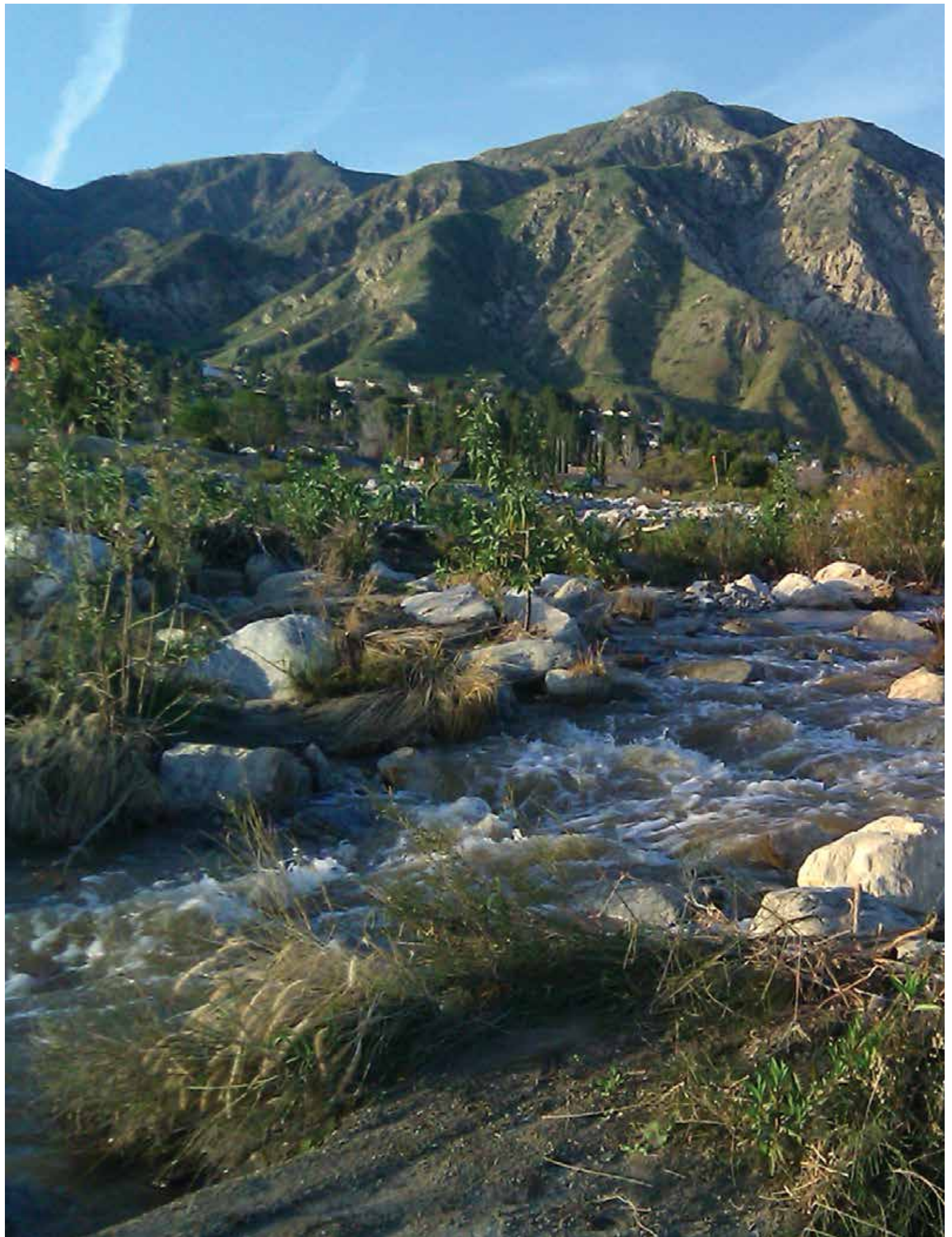


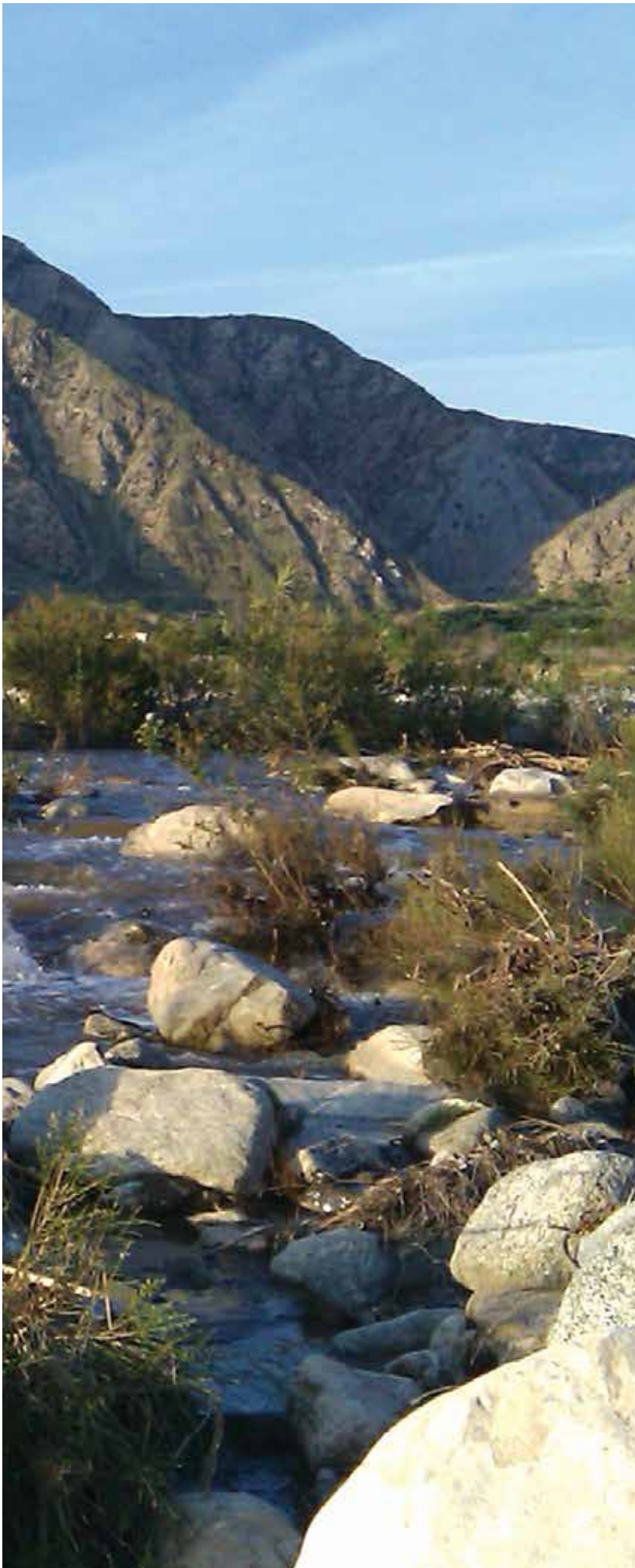
Los Angeles Department of City Planning

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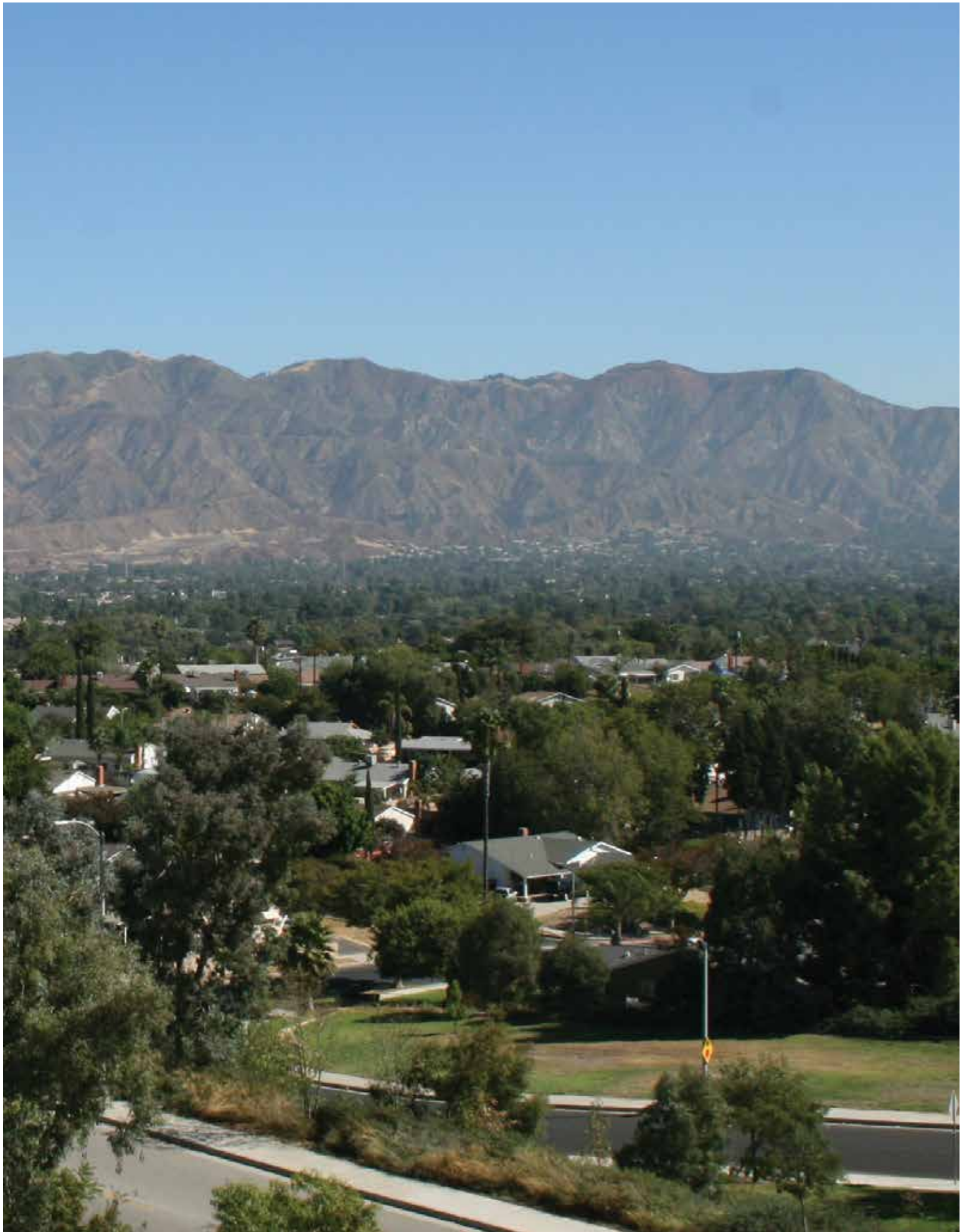
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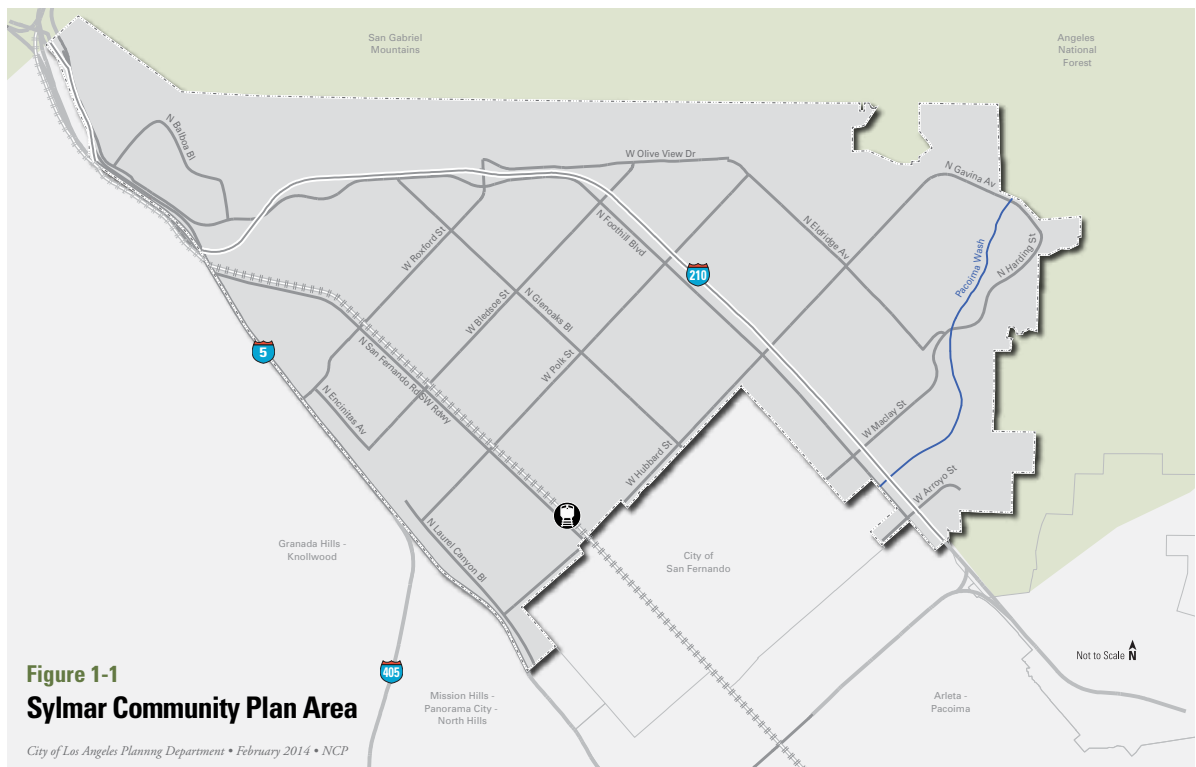
View of the San Gabriel Mountains from the Carey Ranch neighborhood.

Introduction & Orientation

Sylmar is a unique semi-rural suburban community surrounded by scenic mountain views, vast open spaces, and impressive natural beauty. Situated at the foothills of the San Gabriel Mountains in the northeast corner of the San Fernando Valley, this 12-square mile community is located at the northern border of the City of Los Angeles, approximately 28 miles north of downtown, see Figure 1-1, Sylmar Community Plan Area.

The community of Sylmar initially developed as an agricultural outgrowth of the City of San Fernando, with rich soil, an underground water supply, and year round warm climate that supported abundant olive and citrus trees. This community has grown significantly since its olive growing days. Today, Sylmar is a stable semi-rural “bedroom” community at the edge of the City where nearly 80,000 residents appreciate a quiet, slow-paced suburban lifestyle that includes the few remaining equestrian communities in the City. It is a working class community with many multi-generational families, a variety of residential neighborhoods, a mix of commercial uses and services, a thriving light industrial economy, and many natural, cultural, and civic amenities and resources.

The last comprehensive update of the Sylmar Community Plan area was completed in 1997 and since then significant changes have occurred, new issues have emerged, and



new community objectives have evolved. It is necessary to update the Community Plan to reflect current conditions and at the same time plan for upcoming changes. Many changes occurring in Sylmar are caused by larger forces beyond the community's direct control, such as demographic trends, climate change, and economic conditions. Planning for upcoming changes is the most effective and beneficial way for the community to accommodate evolving needs.

The Sylmar Community Plan sets a new direction for the future of Sylmar. Most of Sylmar's neighborhoods are well established and are not expected to change significantly as growth occurs in other parts of the community and throughout the rest of the City. Through a collaborative effort involving residents, owners, businesses, and developers, City staff has produced a Community Plan that sets forth actions to achieve a common vision that encompasses the full spectrum of issues and opportunities regarding Sylmar's physical evolution. The Community Plan addresses a wide range of topics including housing and jobs, parks and open space, urban design, infrastructure, and mobility, as well as arts, culture, history, in addition to serving several important purposes:

- To outline a vision for Sylmar's long-term physical and economic development and community enhancement;
- To provide strategies and specific implementing actions that will allow this vision to be accomplished;
- To establish a basis for judging whether specific development proposals and public projects are in harmony with Plan policies and standards;
- To direct City departments, other public agencies, and private developers to design projects that enhance the character of the community, taking advantage of its setting and amenities; and
- To provide the basis for establishing and setting priorities for detailed plans and implementing programs, including zoning ordinances, design overlays, development standards, the Capital Improvements Program, facilities plans, and redevelopment and other area plans.

The Community Plan's importance lies in its ability to shape positive community change, fostering sustainable land use patterns while balancing the unique character of the community with citywide policies and regional initiatives. The process of developing the Sylmar Community Plan was a multi-year collaborative effort in which broad public input was obtained through a series of meetings and workshops where stakeholders provided input and recommendations.



Many of the residential neighborhoods in Sylmar have stunning views of the San Gabriel Mountains.



Single-family neighborhoods comprise the largest portion of land uses within Sylmar.



El Cariso Regional Park, a beautiful 79-acre park, is a focal point for the surrounding communities, providing amenities from picnic areas, play areas and tennis courts to a community swimming pool.



Industrial parks, like the one on Balboa Boulevard, provide residents with employment opportunities.

Plan Organization

The Sylmar Community Plan is organized into six chapters. Each chapter is further organized into sections that deal with specific topics, described below.

Chapter 1: Introduction and Orientation. Chapter 1 describes how to use the Community Plan, provides a reader's guide for understanding the Plan, and reviews the relationship of the Community Plan to the City's General Plan and other City agencies.

Chapter 2: Community Background. Chapter 2 provides a detailed description of the historical development of the community; describes its relationship to adjoining communities, its environmental setting, and its existing land uses; and identifies population, employment, and housing trends and projections.

Chapter 3: Land Use and Urban Design. Chapter 3 expresses the community's vision for the future, describes the community's land uses, and specifies goals and policies that address residential, commercial, industrial development; design improvements; economic development; jobs/housing balance, historic preservation; the community's equestrian lifestyle; diversity of housing choices; and environmental justice. This chapter also includes urban design guidelines for residential, uses that address more specific design issues for Sylmar.

Chapter 4: Mobility. Chapter 4 defines goals and policies for the community's circulation system, focusing on enhancing mobility and access to all users. Each mode of transportation is discussed, including walking, bicycling, public transit, horseback-riding, and driving.

Chapter 5: Community Facilities and Infrastructure. Chapter 5 discusses key public services and infrastructure, including police, fire, and emergency services, libraries, parks, schools, water, wastewater, solid waste, power (energy), street lighting, and the urban forest. The service provider, existing facilities and service levels, future needs, and issues are identified for each of these facilities or services.

Chapter 6: Implementation. Chapter 6 describes how the Community Plan will be implemented. Each policy in Chapters 3, 4, and 5 is implemented by one or more implementation programs. This chapter describes these implementation programs and identifies the responsible City department or agency.

How to Use the Plan

The Sylmar Community Plan is intended for use by all members of the community, including residents, Neighborhood Councils, business owners, developers, and public officials. The Plan is organized to allow the user to easily find information most relevant to their interest, without perusing the entire document. However, it is important to note that the Plan's policies, guidelines, and implementation programs were not created in isolation, but rather, developed collectively to address community issues in a comprehensive manner.

For residents and Neighborhood Councils, the Community Plan identifies the type and scale of land uses permitted, describes changes that may affect neighborhoods, and explains the goals, policies, design guidelines, and implementation programs that guide decisions about future development.

For businesses, the Community Plan identifies land use measures that support businesses and encourage future success. The Plan includes policies to support and enhance commercial and industrial development. The Plan also discusses land use strategies to attract new investment in commercial centers and corridors.

For developers, the Community Plan introduces the community, provides background information, and establishes development regulations. Developers are encouraged to review all maps, policies, design guidelines, and implementation programs throughout this document to better understand where and what type of development may occur in each part of the community.

For public officials, the Community Plan is a part of the General Plan, which is the basis for land use decisions by the City Planning Commissions, other boards and commissions, and the City Council.

For public agencies, the Community Plan is intended to help agencies contemplate future actions in the City, such as transportation infrastructure improvements, parks, and schools. The policies located throughout the Plan are interrelated and should be examined comprehensively when making planning decisions.

Reader's Guide for Community Plan

The Community Plan is a document that represents the land use vision and values for a distinct geography. A main function of the Community Plan is to guide decision-making with respect to land uses. This includes guidance for legislative decisions, such as adoption of overlay zones or supplemental development regulations, as well as amendments to the land use or zoning maps. The goals and policies, together with the General Plan Land Use Map, are intended to guide decision-making.

Community Plan goals and policies are intended to be supportive of one another. However, it is important to recognize that goals and policies are sometimes in competition and may entail trade-offs. The singular pursuit of one goal or policy may, in some cases, inhibit the achievement of other goals or policies. For example, the Community Plan includes policies that recognize the need to minimize water consumption in light of limited water resources. However, to eliminate the watering of sites being graded for permitted development or to eliminate landscape irrigation may conflict with objectives relating to

Reader's Guide for Community Plan

maintenance of air quality or community design and beautification. Thus, when implementing the Community Plan, decision-makers must strike a balance between competing goals and policies, recognizing that all objectives cannot be fully implemented all the time. In relation to any decision, some goals and policies may be more compelling than others. It is up to the decision-makers to balance and weigh the applicability and merits of the goals and policies on any given project, program, or action.

Ultimately, the Community Plan's goals, policies, programs, and guidelines are intended to provide guidance, and shall be interpreted as directory, unless expressly indicated as mandatory by an asterisk (*). Compliance with the land use General Plan Land Use Map is mandatory.

Format of the Community Plan

Goals

A goal is a statement that describes a desired future condition or "end" state. Goals are change and outcome oriented, achievable over time, though not driven by funding. Each goal in the Community Plan begins with an abbreviated chapter title followed by the number of the goal (e.g. LU1).

Policies

A policy is a clear statement that guides a specific course of action for decision-makers to achieve a desired goal. Policies may refer to existing programs or call for the establishment of new ones. Each policy in the Community Plan is labeled with the abbreviated chapter title, the goal they refer to, and a unique number (e.g., LU1.1). Each policy is followed by its corresponding implementation program(s)(e.g., P1).

Implementation Programs

An implementation program is an action, procedure, program or technique that carries out goals and policies. Implementation programs are comprehensive in nature, encompassing amendments of existing and preparation of new plans, ordinances, and development and design standards; modification of City procedures and development review and approval processes; and interagency coordination. Completion of a recommended implementation program will depend on a number of factors such as citizen priorities, finances, and staff availability. These recommendations are suggestions to future City decision-makers as ways to implement the goals and policies contained in this Community Plan. The listing of recommended implementation programs in the Community Plan does not obligate the City to accomplish them. Chapter 6 contains a list of all the Community Plan's implementation programs. They are grouped by general topic and individually numbered (e.g., P1)

Design Guidelines

The Design Guidelines provide site planning, architectural, and landscape architectural criteria which pertain to different forms of development. The Design Guidelines are intended to encourage well-designed development and quality architecture that enhances existing neighborhoods, creates identity, and promotes a positive physical image. The Design Guidelines are intended to provide further detail as to the desired features of buildings and structures, additions to existing structures, and new construction, consistent with Community Plan policies. The Design Guidelines are not compulsory but rather are intended to be useful in determining General Plan compliance. Guidelines appear throughout Chapter 3, grouped by general topic and individually numbered (e.g., G1).

Relationship to the General Plan

California State law requires that cities prepare and adopt a comprehensive, integrated, long-term General Plan to direct future growth and development. The General Plan is the fundamental policy document of a city. It defines how a city's physical and economic resources are to be managed and utilized over time. Decisions by a city with regard to the use of its land, design and character of buildings and open spaces, conservation of existing and provision of new housing, provision of supporting infrastructure and public and human services, and protection of residents from natural and man-caused hazards are guided by and must be consistent with the General Plan.

State law requires that the General Plan contain seven elements: land use, transportation, housing, conservation, open space, noise, and safety. Cities may also choose to incorporate additional elements to more directly address locally significant issues. There must be internal consistency among the elements. In Los Angeles, the thirty-five Community Plans, including the Sylmar Community Plan, comprise the City's land use element, see Figure 1-2, City of Los Angeles Community Plan Areas. In addition, the City has adopted an overarching "Framework Element" discussed below.

General Plan Framework Element

The City's General Plan Framework Element is the citywide plan that establishes how Los Angeles will grow in the future. Adopted in 1996, and re-adopted in 2001, the Framework Element is a strategy for long-range growth and development, setting a citywide context for the update of Community Plans and citywide elements. The Framework Element responds to state and federal mandates to plan for the future by providing goals, policies, and objectives on a variety of topics, such as land use, housing, urban form, open space, transportation, infrastructure, and public services. Many of the Framework Element's key guiding principles, summarized below, can be advanced at the community level via Community Plans.

Citywide General Plan Elements

- Open Space
- Housing
- Transportation
- Conservation
- Land Use
- Noise
- Safety
- Infrastructure
- Public Facilities and Services
- Framework



Figure 1-2
City of Los Angeles
Community Plan Areas
Sylmar Community Plan Area

This is an information map provided for reference purposes only. It is not adopted as a part of the Community Plan.

Framework Element Guiding Principles

Grow strategically. Should the City's population continue to grow, as is forecast by the Southern California Association of Governments (SCAG), growth should be focused in a number of higher-intensity commercial and mixed-use districts, centers, and boulevards, particularly in proximity to transportation corridors and transit stations. This type of focused growth links development with available infrastructure and encourages more walkable, transit-friendly neighborhoods, helping to ease our reliance on the automobile, and minimizing the need for new, costly infrastructure.

Conserve existing residential neighborhoods. By focusing much of the City's growth in centers and along commercial corridors, the City can better protect the existing scale and character of its single- and multi-family neighborhoods. The elements that contribute to the unique character of different residential neighborhoods should be identified and preserved whenever possible.

Balance distribution of land uses. Maintaining a variety of land uses is crucial to the long-term sustainability of the City. Commercial and industrial uses contribute to a diverse local economy, while residential uses provide necessary housing for the community. Integrating these uses within smaller geographical areas can better allow for a diversity of housing types, jobs, services, and amenities.

Enhance neighborhood character through better development standards and guidelines. Better development standards and guidelines are needed to both improve and enhance existing neighborhood character and ensure high-quality design in all types of development. In 2011, the City Planning Commission adopted citywide guidelines for commercial, residential, and industrial projects. In addition, many communities have design regulations in certain key geographic areas, as well as, design guidelines in the Community Plan for particular types of development.

Improve the connection of public and private spaces through good urban design. Good urban design improves the relationship between private development and the public realm. The placement of architectural features, windows, entrances, walkways, street trees, landscaping, and lighting all help to establish either a positive or negative interaction between a building and its surroundings. Good urban design practices help to create successful public and private spaces where people feel comfortable and that foster a sense of community.

Create more small parks, pedestrian districts, and public open space.

While regional parks and green networks are an important component of the City's open space strategy, more small-scale, urban open spaces must be developed as well, as they are crucial to the quality of life of the City's residents. There are many opportunities at the community level to create public "pocket" parks or mini parks as part of new developments, to enhance pedestrian orientation in key commercial and residential areas, and to build well-designed public plazas and gathering places.

Improve mobility and access. The City's transportation network should provide adequate access to jobs, services, amenities, open space, and entertainment, and maintain acceptable levels of mobility for all those who live, work, travel, or move goods in Los Angeles. Attainment of this goal necessitates a comprehensive program of physical infrastructure improvements, traffic systems and traffic demand management techniques, and land use and behavioral changes that reduce vehicle trips. An emphasis should be placed on providing for and supporting a variety of travel modes, including walking, bicycling, horseback riding, public transit, and driving.

Identify a hierarchy of Districts, Centers, and Boulevards. The Framework Element provides an overall structure and hierarchy for the City's commercial areas. This hierarchy, described in more detail below, helps us understand the roles that different types of commercial areas play within our communities so that we can better foster their unique characteristics. Our City's commercial areas serve a variety of roles and functions, from small neighborhood gathering places with local cafes and shops to major job centers and entertainment hubs. Although these areas are typically designated for commercial use, they often contain residential and mixed-use buildings as well. Commercial areas are grouped into four general categories: Neighborhood District, Community Center, Regional Center, and Mixed-Use Boulevards.



The Metrolink Station located in Sylmar serves as a multi-modal transit terminal and transfer point for bus riders.

Framework Element Hierarchy

The City's commercial areas are grouped into four general categories: Neighborhood Districts, Community Centers, Regional Centers, and Mixed Use Boulevards. Some Community Plan areas do not contain all four types of districts or centers.



Abbot Kinney Boulevard in Venice

Neighborhood District. A Neighborhood District is a focal point for surrounding residential neighborhoods and contains a diversity of land uses that serve the daily needs of these residents and employees. Local businesses and services often include restaurants, retail outlets, grocery stores, child care facilities, small professional offices, community meeting rooms, pharmacies, religious facilities and other similar services. The clustering of these types of uses and the frequency of their location near neighborhoods are intended to encourage walking and bicycling to and from adjacent neighborhoods, minimizing the need for automobile trip-making. Neighborhood Districts are typically characterized by small-scaled development and a pedestrian-orientated character.

Community Center. Community Centers differ from Neighborhood Districts in their size and intensity of business and social activity. While they typically include the types of businesses and services found in Neighborhood Districts, they also contain uses that serve the larger community, like hotels or motels, small offices, cultural and entertainment facilities, and schools and libraries. Generally, Community Centers are medium-scaled, although this varies depending on the character of the surrounding area. Community Centers are often served by small shuttles, local and rapid buses, or subway stops.



Hollywood and Western Avenue



Century City

Regional Center. A Regional Center is a hub of regional commerce and activity and contains a diversity of uses such as corporate and professional offices, residential buildings, retail commercial malls, government buildings, major health facilities, major entertainment and cultural facilities, and supporting services. Regional Centers cater to many neighborhoods and communities and serve a much larger population than either Community Centers or Neighborhood Districts. They are generally high-density places whose physical form is substantially differentiated from the lower-density neighborhoods of the City. They typically provide a significant number of jobs, but are also non-work destinations as well. As a result of their densities and functions, Regional Centers are usually located near major transportation hubs or along major transportation corridors. There are no Regional Centers identified in the Sylmar Community Plan.

Mixed-Use Boulevard. Mixed-Use Boulevards serve as the "connecting spaces," linking Neighborhood Districts, Community Centers, and Regional Centers with one another. The scale, density, and height of development along designated Mixed-Use Boulevards vary throughout the City, but are intended to be compatible with adjacent residential neighborhoods. The term "mixed-use" connotes a variety of uses occurring within the boulevard, but also the potential for mixing uses within individual structures, such as commercial on the ground floor and residential above. Mixed-Use Boulevards should provide community and neighborhood commercial uses, public services, cultural facilities, school classrooms, and similar facilities to residents and employees within walking distance of surrounding residential neighborhoods and accessible from the boulevard's public transit.



Downtown Los Angeles

Relationship to Other Agency Plans

A variety of agencies and organizations influence development and land use decision-making in the Community Plan area, see Figure 1-3, Other Agency Plans. In each case, the plans and use of property by other agencies must be consistent with the Community Plan. This required consistency holds true for redevelopment and capital improvement programs and projects, development entitlements, and other actions pertaining to the City's physical development. Relevant agencies and plans in Sylmar include:

Community Redevelopment Agency of Los Angeles (CRA/LA)

CRA/LA, a Designated Local Authority (DLA), is the successor to the former Los Angeles Community Redevelopment Agency, the public agency established in 1948 pursuant to California State Law (Code Section 33000), to attract private investment into economically depressed communities. Although ABX1-26 dissolved the Agency in 2012, the land use authorities granted in the Redevelopment Project Area Plans remain effective and will continue to be administered by the DLA.

The Earthquake Disaster Assistance Project Area in Sylmar includes portions along San Fernando Road, Hubbard Street, and Foothill Boulevard. The Project was established in 1994 as a result of the Northridge Earthquake. While this Plan is set to expire in 2015, efforts are underway to expand its boundaries and extend its life, so that efforts to stimulate economic development in this part of Sylmar can continue.

Los Angeles Community College District (LACCD) LACCD consists of nine colleges, including the Los Angeles Mission College (LAMC) which is located within the Sylmar CPA. LAMC has prepared the 2009 Facilities Master Plan to help guide the development of the instructional and support facilities needed to accommodate increased enrollment through 2015. However, City Planning approval is not required prior to LAMC obtaining necessary permits. See Figure 5-1, Public Facilities.

Los Angeles County Metropolitan Transportation Authority (Metro) Metro is the state chartered regional transportation planning and public transportation operating agency for the County of Los Angeles. The agency develops and oversees transportation plans, policies, funding programs, and both short-term and long-range solutions that address the County's increasing mobility, accessibility and environmental needs. Metro operates the Sylmar/San Fernando Metrolink Station located in the southwest corner of the community.

The Angeles National Forest (ANF) ANF was established by Executive Order in December 1892. It covers about 700,000 acres and is the backyard playground to Sylmar and the greater Los Angeles area. ANF manages the watershed within its boundaries and provides valuable water to Southern California and protects surrounding communities from catastrophic floods. ANF is located in the San Gabriel Mountains of Los Angeles County.

Los Angeles Unified School District (LAUSD)

LAUSD currently operates several K-12 schools in Sylmar, including one high school. In addition, LAUSD operates early childhood education centers, adult schools, and a continuation program within Sylmar. The LAUSD develops an annual Planning and Development Branch Strategic Execution Plan, which describes goals and progress for school site planning. However, City Planning approval is not required prior to LAUSD obtaining necessary permits. See Figure 5-1, Public Facilities.

Santa Monica Mountains Conservancy (SMMC)

SMMC was established by the California State Legislature in 1980 to help create and preserve parkland in both wilderness and urban settings. The SMMC Zone covers an area from the edge of the Mojave Desert to the Pacific Ocean, including the San Gabriel Mountains and the Rim of the Valley Trail Corridor which are in the northern portions of Sylmar. The Sylmar Community Plan seeks to coordinate efforts to preserve and protect parkland, watershed, and open spaces by providing adequate buffers and transitional uses between the foothills and the rest of the community.

State Enterprise Zone (SEZ)

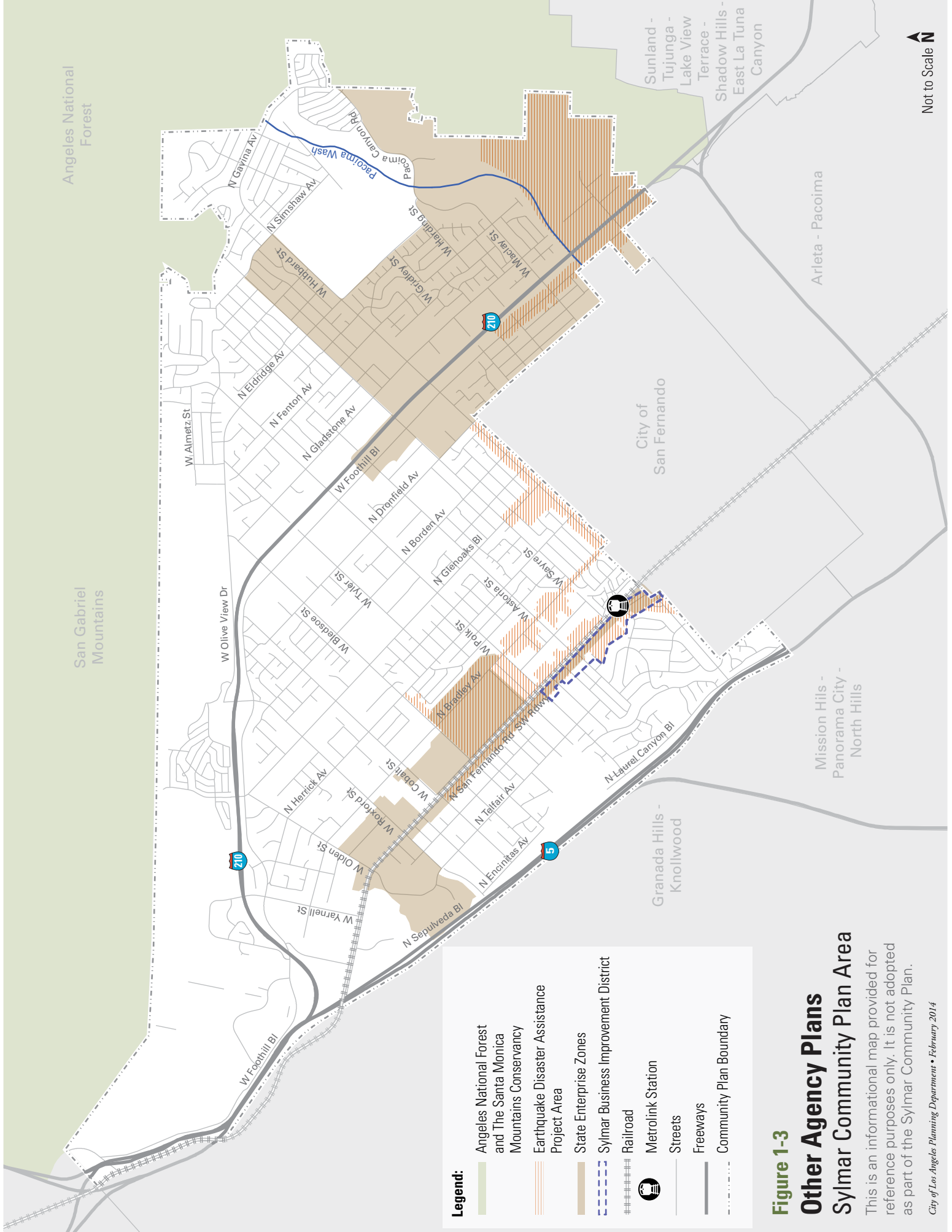
SEZ program targets distressed areas throughout California, providing special incentives designed to encourage business investment and promote the creation of new jobs. A portion of the southeast corner of Sylmar is within the boundaries of the East Valley Region of the Los Angeles State Enterprise Zone. Businesses in this enterprise zone can take advantage of special State tax credits and deductions that are intended to stimulate business attraction, growth, and retention, and increased employment opportunities within Sylmar.

Sylmar Property and Business Improvement District (S-PBID)

S-PBID is a public-private partnership established in 2007 to fund improvements within the district's boundaries. It consists of property owners and commercial tenants who are dedicated to promoting business development and improving the area's quality of life. The district is located within a three-quarter mile area along San Fernando Road between Polk and Hubbard Street. Business Improvement Districts deliver supplemental services such as capital improvements and beautification for the area – all funded by a special assessment paid by property owners within the district.

The United States Army Corps of Engineers (USACE)

USACE is one of the primary agencies responsible for flood control. There are several flood control channels within Sylmar, such as the Pacoima Wash. Permits are required for work or structures within navigable waters, discharge of dredged or infill material into waters, and many water bodies, such as the Pacoima Wash, and wetlands are subject to USACE's regulatory authority. The proposed playing fields near the Pacoima Wash are subject to its regulatory authority.



Not to Scale

- Legend:**
- Angeles National Forest and The Santa Monica Mountains Conservancy
 - Earthquake Disaster Assistance Project Area
 - State Enterprise Zones
 - Sylmar Business Improvement District
 - Railroad
 - Metrolink Station
 - Streets
 - Freeways
 - Community Plan Boundary

Figure 1-3
Other Agency Plans
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

City of Los Angeles Planning Department • February 2014

Special Districts

One of the primary methods of implementing the Community Plan is through zoning regulations. Special districts or overlays allow zoning regulations to be tailored specifically to the community, taking into account geographic features, architecture, history and unique character or design features. As shown in Figure 1-4, Special Districts, several such districts or overlays that influence decision-making in the community include:

Sylmar Equinekeeping “K” District

The Sylmar Equinekeeping “K” District, amended concurrently with this Community Plan, is an overlay ordinance that preserves the equestrian lifestyle on residential lots in the north and central areas of Sylmar by maintaining minimum lot size, equinekeeping lot features, adequate distance to non-equinekeeping neighbors, as well as assisting in access to and development of equestrian trails. This “K” District includes a set of conditions in addition to those in the L.A.M.C. Equinekeeping “K” District section. The Community Plan Design Guidelines also include specific guidelines for equinekeeping neighborhoods.

Sylmar Community Plan Implementation Overlay (CPIO) District

The Sylmar CPIO provides standards for development projects, including new development and improvements to existing properties, within the community. The intent of the CPIO is to enhance the identity of the community and to improve the walkability and appearance of neighborhood districts, commercial areas, and multiple family residential areas near public transit.



Not to Scale

Figure 1-4
Special Districts
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted part of the Sylmar Community Plan.



Tract housing increasing in the Valley (1957). Courtesy of Hollywood Citizen News/Valley Times Collection/Los Angeles Public Library.

Community Background

Once the site to the world's second largest olive groves, today Sylmar is a quaint bedroom community with a variety of services and amenities nestled in the foothills of the San Fernando Valley.

The earliest recorded inhabitants of the San Fernando Valley were the Tongva Indians, also known as the Gabrielinos. In 1797, a group of Spanish expeditioners, led by Gaspar de Portola, built the Mission San Fernando Rey de Espana, three miles south of Sylmar and the Mission Wells and Settling Basin. By the turn of the century the Mission had become a small, thriving center of trade where fruits, vegetables, olives, wine, and livestock were bought and sold. The Mission was the center of regional activity in the Valley until the City of San Fernando, located near the Southern Pacific Railroad tracks, was founded in 1874 by Charles Maclay. The railroad line, which traverses the western sector of the community, was completed in 1876 and linked San Fernando with downtown Los Angeles and San Francisco. The completion of the railroad tracks connected the valley with the rest of the nation, and was crucial to Sylmar's early development. Figure 2-1, Community Structure, highlights key points of interest in and around Sylmar.

By the mid-nineteenth century, smaller independent towns began emerging around the City of San Fernando, including Morningside — the community now known as



Indian Women at the San Fernando Rey de Espana Mission (1890). Courtesy of C.C.Pierce & Co./Los Angeles Public Library.



Mission Wells and Settling Basin, Los Angeles Historic-Cultural Monument No. 50.

Sylmar. The area developed as an agricultural outgrowth of the City of San Fernando. In 1886, a group of Illinois businessmen purchased 2,000 acres of land, east of the railroad tracks on San Fernando Road and south of Roxford Street, and planted over 1,000 acres with olive trees. The nutrient-rich soil, underground water supply, and year round warm temperatures produced some of the world's finest olives and olive oil. During this period, Sylmar was considered to have the second largest olive grove and mill in the world. The packaging plant located near the railroad tracks on San Fernando Road and Roxford Street continued to sell olives and olive oil under the Sylmar Packing Label until 1925.

The completion of the Los Angeles Aqueduct in 1913, stretching 233 miles from the Owens River to the Los Angeles region, transformed Los Angeles, as it brought water to the City through the terminus at the northwest corner of the community that is now Sylmar. The area of Morningside was shortly renamed Sylmar ("sea of trees") after incorporation into the City of Los Angeles in 1915. Most of the northern rural areas in the valley were also annexed into the City around the same time in order to gain access to the new water supply, more than doubling the size of Los Angeles. With a new stable supply of water, combined with near perfect weather and soil conditions, Sylmar was an ideal place to cultivate farms and produce commodities and for many years flourished as a strong farming community.

For most of the 1920s and 1930s, Sylmar produced olive oil, pickled olives, and varietal flowers. Horse ranching and poultry farming were also historically part of the economic life of the area. The barns and other structures that survive provide a strong visual characteristic of the early history of Sylmar. Stetson Ranch is the most notable ranch in the area, though no early buildings or farm structures remain. The limited number of architectural resources from each period of Sylmar's history indicate that the community remained rural until the 1950s.



Los Angeles Aqueduct Dedication (1913). Courtesy of Herald-Examiner Collection/Los Angeles Public Library.



Sylmar Packing Corporation (1950). Courtesy of Hollywood Citizen News/Valley Times Collection/Los Angeles Public Library.

The end of World War II and the ensuing “baby boom” accelerated urbanization and population growth in the Valley and the rest of the City. The demand for growth that followed the mid-twentieth century spurred urbanization and effectively eliminated the olive industry and the small agricultural farms located in Sylmar. It was during this period, between 1940 and 1960 that much of the community’s housing stock was built, with single-story ranch-style homes that still predominate in many neighborhoods. During this time, the first major shopping center was built on Foothill Boulevard and the two major freeways (the Foothill and the Golden State) that traverse the community began construction, connecting Sylmar with the rest of the City and attracting new industries to Sylmar.

In conjunction with the new levels of urbanization, the Sylmar Branch Library opened in 1961. The first public park was formally dedicated in 1964 on the site of the old El Retiro School for Girls at Polk Street and Borden Avenue and is now the Sylmar Recreational Park, the only community Park in Sylmar. In 1969, the Hang Gliders Flight Park opened west of the Pacoima Wash, offering an alternative site for residents seeking recreational space north of the Foothill Freeway.

The 1970s were “rebuilding” years, as efforts moved forward to rebuild or transform structures that were destroyed in the 1971 Sylmar Earthquake. The destroyed Veterans Administration Hospital became the Veterans Memorial Park and the former tuberculosis sanitarium was rebuilt as Valley Care Olive View-UCLA Medical and Health Center, the only county hospital in the North Valley. In 1975, the Foothill Freeway (I-210), the 79-acre El Cariso County Regional Park and Golf Course, and the 11-acre Los Angeles Mission College were also completed. In 1978, the Los Angeles County Juvenile Hall was constructed, one of the few juvenile centers in the Valley. Three new mobile homes parks opened in the northwest area of the community in the late 1970s, and many shopping centers, and industrial developments, such as distribution warehouses, also began emerging east of the railroad tracks.

In the 1980s, Sylmar experienced some of the highest population and housing growth



Olive View Medical Center (1970). Courtesy of The Valley Relics Collection.



Olive Groves on Roxford Avenue (1937). Courtesy of the Los Angeles Public Library.

rates in the City. The community grew by nearly twenty thousand residents and more than 4,500 residential units were built within a decade. Sylmar also transitioned from an Anglo to a Latino majority population. A number of large condominiums, and commercial, and industrial developments were constructed filling in most of the un-built land along major corridors and near the freeways.

By the 1990s, growth rates had tapered off. Most of the development took place along the periphery of the community where large parcels still remained undeveloped. Several industrial parks and planned residential neighborhoods were constructed during this time period. In 1993, the Sylmar/San Fernando Metrolink Station was built linking Sylmar to Lancaster and downtown Los Angeles and providing connections to Metro, LADOT, and the Santa Clarita Transit bus systems. In 1994, Sylmar was struck by another earthquake, the Northridge Earthquake, damaging large portions of its infrastructure.

Today, Sylmar is a relatively established semi-rural suburban community with a variety of services and amenities. Most of the recent residential activity has entailed remodeling or upgrading existing homes and/or subdividing large lots into small, more compact single and multi-family housing. The nearly completely destroyed Oakridge Mobile Home Park is seeing signs of rebirth after the fire of 2008; with a new community center and homes constructed. Interest in redeveloping Sylmar's commercial and industrial districts and public facilities has gained momentum in recent years. Several new mixed-use housing, retail, and office projects have been entitled near the Sylmar/San Fernando Metrolink station, existing shopping centers have been remodeled, and new industrial businesses have opened. San Fernando Road has begun to see improvements; with a new bike lane and street trees, and a new landscaped median. In addition, the Los Angeles Unified School District has upgraded many of its facilities and the Los Angeles Mission College has expanded its campus adding new facilities and amenities.

Over the next decade, the focus of development in Sylmar will be to enhance those elements and features of the community that contribute to its overall sense of charm, character, and function while directing growth strategically so that new development is compatible with existing land uses and scale and preserve the natural environment. Established residential neighborhoods will be protected and are not expected to change significantly as growth occurs in other parts of Sylmar and targeted locations in the City. New housing will be directed towards the Sylmar/San Fernando Metrolink Station and other major corridors. The community's local economy will be protected by preserving existing industrial and commercial land and by providing new opportunities to expand where appropriate and feasible. This strategic and balanced approach will not only protect residential neighborhoods and the natural resources, but will provide new housing, commercial, and industrial opportunities in areas that have the infrastructure and amenities in place to support growth.



Multiple family housing in Sylmar, located along Foothill Boulevard.



The North Valley Business Center, located on Telfair Avenue, is one of the main industrial parks within the community of Sylmar, providing employment opportunities.



A pleasant bicycle path, shaded by street trees along San Fernando Road.



Oakridge Mobile Home Park resurges after devastation caused by the 2008 wildfires.



Existing Land Uses

Sylmar is a semi-rural suburban community framed by open space, as illustrated in Chart 2-1, Land Use Distribution. Its natural and man-made boundaries have helped define the area and have influenced its development pattern. It is bounded by the San Gabriel Mountains and Angeles National Forest to the north and east, the Pacoima Wash to the southeast, the City of San Fernando to the south, the Golden State Freeway (I-5) and San Diego Freeway (I-405) to the west, and is traversed by the Foothill Freeway (I-210) through the northeast portions of the community. The community of Sylmar is relatively built-out with a mixture of single and multiple family residential, commercial, industrial, civic, passive and recreational uses, and public facilities within a “sea of trees.”

Residential uses comprise the largest portion of land uses within Sylmar, with 4,380 acres or 64 percent of the community designated for residential uses. More than half, 57 percent, is designated for single-family uses, with the remaining 7 percent designated for multi-family uses. Single-family residential uses are located throughout the community. Most of the single-family neighborhoods are characterized by single-story ranch style homes that were built post World War II with more recent development characterized by two-story modern and post-modern architecture. The oldest single-family neighborhoods are located in the central portion of the community featuring semi-rural characteristics such as large lots with horsekeeping, stables, and equestrian trails that lead into the national forest.

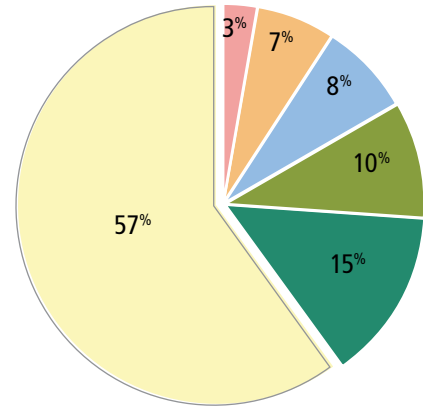
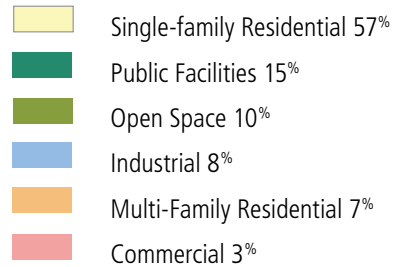


Chart 2-1

Land Use Distribution (2006)



Source: City of Los Angeles
Department of City Planning, 2010.



Typical single-family residential neighborhood in Sylmar.



The small-scale, two-story buildings with individual front entrances are characteristics of many of the apartments and condominiums on Foothill Boulevard.



Typical single-story commercial development in Sylmar.



Streets lined with trees is characteristic of many Sylmar industrial districts.

Multiple family uses consist of nearly seven percent or about 484 acres of the land in Sylmar. Most of the multi-family neighborhoods are concentrated in the central west portion of the community along Foothill Boulevard and in the south east portion near the Sylmar/San Fernando Metrolink Station, with several residential neighborhoods along Hubbard Street, San Fernando Road, and along the fringes of the community. The older neighborhoods comprise of a mix of two and three-story attached and detached apartments and condominiums with newer development varying from two-story townhomes to four-story apartment buildings. There are also several mobile home parks located throughout the community.

Commercial land uses comprise less than three percent or about 195 acres of the land in Sylmar. Most of this land is located in the southern portions of the community along the commercial corridors of Foothill Boulevard, Glenoaks Boulevard, Gladstone Avenue, and Laurel Canyon. These corridors consist of shopping centers containing a mix of retail, office and financial services, auto-related uses, and general commercial services and uses. Several smaller neighborhood commercial districts are located throughout the community at major intersections along Glenoaks Boulevard, Roxford Street, and Foothill Boulevard. These districts provide a variety of uses that serve the immediate local residential neighborhoods. There are also two mixed-use boulevards along San Fernando Road and Maclay Avenue which offer a variety of commercial uses and residential units.

Industrial land use comprises approximately 530 acres or eight percent of the land in Sylmar. Industrial uses are concentrated in the central west portion of the community along the railroad tracks and in the northwest and southeast corners of the community near major freeways. Sylmar has two light industrial districts, one located in the southeast corner of the community near the I-210 freeway and the other in the central west portion of the community near the railroad tracks. The uses located in these districts contain a mix of distribution and warehouse uses and industrial services such as machine and print shops. In addition, two industrial parks are located in the western portion of the community near the I-5 freeway. The uses in these parks primarily consist of research and development with some distribution uses. A small cluster of industrial uses is located on the western edge of the community containing primarily warehouse uses.

Open space comprises 676 acres or ten percent of the community. The largest open space area is located in the eastern portion of the community, north of the I-210 freeway, and includes El Cariso Community Regional Park and Golf Course, the Pacoima Wash, Hanggliders Flight Park, Sylmar Independent Baseball League, and Veterans Memorial Community Regional Park. Additional open space is located in the northern portion of the community, abutting the San Gabriel Mountains and Angeles National Forest. This area includes two specialty parks that serve the equestrian community, Stetson Ranch and Wilson Canyon Parks. These parks provide trailheads that lead up to the Rim of the Valley Trail Corridor and the Angeles National Forest. In addition, a community park is centrally located near three public schools and a neighborhood park is located in southwest corner of Sylmar. Other open space areas include the historically designated Mission Wells and Debris Basin located in the southwest corner, the Pioneer Cemetery centrally located in the community, and the two flood control channels that traverse the community.

Public Facilities, such as schools, fire stations, hospitals, and utilities, is the second largest land use designation in the community representing 1,046 acres, or 15 percent. Olive View-UCLA Medical Center is the largest public facility with Los Angeles Mission College being the second largest public facility in Sylmar, both located north of the I-210 freeway. The third largest public facility is a Department of Water and Power facility that consists of a power receiving station, several water storage containers, and the transmission line right-of-ways that run parallel to the I-210 freeway and bisect the community in its western portion. Other public facilities include: the juvenile detention facility located in the northwest corner of the community, the library and fire station centrally located on Polk Street, and 10 public schools located throughout the community.



Parks, open space, and public facilities, such as the Lakeside Debris Basin, offer recreational opportunities throughout Sylmar.



Stetson Ranch, located at the base of the Angeles National Forest in Sylmar, is a shady rural park that offers hiking and equestrian trails, as well as equestrian staging areas.



Sylmar has been called “the world capital of hang gliding” and pilots have been flying hang gliders over the area since 1969.



Olive View-UCLA Medical Center is a hospital, funded by Los Angeles County, and is one of the primary healthcare delivery systems in the north San Fernando Valley.

Relationship to Adjacent Communities

The community of Sylmar is located in the northeast section of the San Fernando Valley at the southern foot of the San Gabriel Mountain range. Adjacent communities within the City of Los Angeles include Granada Hills–Knollwood and Mission Hills–Panorama City–North Hills and the incorporated City of San Fernando. Sylmar shares the southern boundary with the City of San Fernando and much of its history, identity, and ethnic community. The City of San Fernando and nearby cities, such as Burbank and Santa Clarita, provide retail and entertainment opportunities not found locally. In exchange, Sylmar employs many blue and white collar workers that reside in nearby communities. Sylmar also offers a variety of recreational activities, such as horseback riding, hang gliding, and hiking opportunities. It also provides higher educational opportunities, health services, and correctional facilities to nearby communities. Lastly, Sylmar offers rail services to nearby residents traveling locally and regionally.

Trends and Projections

The State of California requires that cities plan for changes in population, housing, and employment; if growth is projected, each city must accommodate a share of the region's anticipated growth. These projections are developed by the Southern California Association of Governments (SCAG), which forecasts population and job growth for the cities and counties in the six-county Southern California region. The City must then accommodate, or create the "capacity" for, these projected levels of population, housing, and employment through its Community Plans. This section describes the Sylmar Community Plan's population, housing, and employment projections, as well as other influencing factors that may impact these estimates. In addition, recent state legislation, including two important climate change bills, is discussed.

Population, Housing and Employment

SCAG's 2030 population and housing forecasts for Los Angeles are based on historic and recent growth trends. The Department of City Planning (DCP) refines the population and housing allocations within the City's 35 communities so that projected growth is directed to regional and commercial centers, consistent with the Framework Element and other City policies.

The Sylmar Community Plan is designed to accommodate the 2030 population, housing, and employment projections based on assumptions about the amount of development that can reasonably be expected to occur during the life of the plan, given the Community Plan’s land use designations and policies. Past building data demonstrates that many sites will not be built to the maximum densities permitted by the Community Plan for a variety of reasons, including economic conditions and market trends, financial lending practices, construction and land acquisitions costs, physical site constraints, and other General Plan policies or regulations. These reasonable expectations about the level of future development determine the Community Plan’s capacity to absorb any increase in population, housing, and employment. The 2030 projections and plan capacity for population, housing units, and employment in Sylmar are shown in Table 2-1, Population, Housing and Employment Projections and Capacity for Sylmar.

**TABLE 2-1
Population, Housing, and Employment Projections
and Capacity for Sylmar**

	Existing (2005 Estimate) ¹	2030 Projection	Plan Capacity
Population (persons)	71,794	85,9932	88,008
Housing (dwelling units)	18,329	23,8872	24,438
Employment (jobs)	19,619	25,6603	26,453

¹ Southern California Association of Governments (SCAG), 2005 estimate.
² City of Los Angeles Department of City Planning, adjusted SCAG projection.
³ SCAG projection.

Other Influencing Factors

In any planning effort, population projections and estimates are prepared in an attempt to anticipate, predict, and forecast population trends over a planning period. Understanding population change is necessary to predict future demand for housing units, land area for future development, transportation demand, community facilities and natural resources within the Plan area. It needs to be recognized, however, that these figures are only best estimates and are derived from regional data disaggregated to the city and community level. The intensity of development is affected by many factors and the rate at which population, jobs and housing grow may be faster or slower than anticipated. External factors, such as global economic trends, demographic changes, immigration and migration, global warming, and water rights and related litigation may also influence community development. These factors are difficult to quantify and are often beyond the control of local jurisdictions, but are important considerations as future trends are anticipated, predicted, and forecasted.

State Legislation

At the state level, Senate and Assembly bills are often adopted that influence local planning policy. The Sylmar Community Plan includes new policies and programs that address these important objectives. For example, legislation calls for greater local emphasis on greenhouse gas reductions as well as better integration of transportation and land use planning:

Global Warming Solutions Act of 2006 (Assembly Bill 32) requires California to reduce its greenhouse gas emissions to 1990 levels by no later than 2020. The California Air Resources Board (CARB), as the State’s lead air pollution control agency, was assigned primary responsibility for coordinating development of those measures needed to achieve the required emissions reductions.

Complete Streets Act of 2007 (Assembly Bill 1358) requires cities when updating its General Plan, to provide for the routine accommodation of all users of the roadway including motorists, pedestrians, bicyclists, individuals with disabilities, seniors, and users of public transportation.

Landmark Land Use and Greenhouse Gas State Law of 2008 (Senate Bill 375) helps to implement Assembly Bill 32’s greenhouse gas reduction goals by targeting transportation-related emissions through better integration of land use and transportation planning.



Land Use & Design

Sylmar serves as a bedroom community where residents appreciate a quiet, slow-paced, semi-rural suburban lifestyle that offers close proximity to amenities and jobs within the community and the greater San Fernando Valley and provides needed resources and amenities to the city and region. The focus of development in Sylmar will be to enhance those elements and features of the community that contribute to its overall sense of charm, character, function, and livability, while directing growth strategically so that new development is compatible with existing land uses and scale and protects the natural environment.

This Chapter provides guidance regarding the ultimate land use pattern and development for Sylmar. Based on Elements of the City of Los Angeles' General Plan, this Chapter formulates a coherent set of goals, policies, and design guidelines to guide growth in a manner that helps preserve, protect, and enhance existing natural, historic, architecture, and cultural resources. These land use policies and design guidelines serve as the central organizing element for the Community Plan, providing strategies for accomplishing the Community Plan's vision. This chapter is organized into four general categories:

- Residential
- Commercial
- Industrial
- Historic, Architecture, and Cultural Resources

Goals and policies for community facilities and infrastructure are included in Chapter 5. The programs to implement the Plan's goals, policies, and design guidelines are included in Chapter 6.

Sylmar Vision Statement

To achieve the goal of a sustainable future with a high quality of life, the Sylmar Community Plan includes the following Vision Statement that describes what the community seeks to become — how it will look, function, and how it might be better or different in the future. The Vision Statement gives the plan a purpose and provides a basis for its development. The vision for Sylmar is for a stable community that provides a high quality of life for its residents: one that builds upon its distinct natural beauty and resources, rich agricultural and cultural heritage, and proximity to various interstate highways and public transit. Sylmar envisions a community that:

- Maintains a semi-rural suburban atmosphere, as a primarily “bedroom” community, by sustaining and protecting residential areas, increasing and enhancing its recreational areas, public facilities and services, and improving its commercial and industrial areas;
- Promotes a conservative plan which preserves existing single-family residential neighborhoods and accommodates a variety of housing opportunities near public transit, services, and amenities at densities that complement existing neighborhoods and designs that create appropriate transitions from one housing type to another;
- Preserves the rustic charm and equestrian lifestyle of the area by protecting equestrian-oriented neighborhoods, incorporating and developing trails, and retaining historic/cultural resources and significant features;
- Is carefully planned and balanced, allowing for an increase in urban conveniences and opportunities without overpowering the semi-rural suburban community they serve;
- Features sustainable growth that supports and revitalizes commercial areas and transit-oriented, mixed-use corridors in need of physical and economic improvements, and which encourages pedestrian activity and use of public transit.
- Enhances and preserves existing commercial areas ensuring there is an adequate level of commercial services provided throughout the Plan area;
- Supports environmentally sensitive industrial and employment centers that provide local and regional jobs and bolster the community’s economic and physical condition;
- Preserves as much of the natural terrain and environment as possible through thoughtful development of the areas near the foothills and the Pacoima Wash. The conservation of natural terrain and open space areas in the foothills north of the Foothill Freeway are especially important, since they are part of the Tujunga Watershed; and
- Supports an adequate circulation system that will allow traffic to flow on major streets and create a safe and landscaped atmosphere for pedestrians, bicyclist, and equestrians.

Vision Statement

The Vision Statement is unique to the Sylmar Community Plan, and provides a foundation for change that is shared by community members, homeowners, developers, business owners, elected officials, and City departments.



The San Gabriel Mountains serve as a backdrop to the semi-rural suburban neighborhoods in Sylmar.



Quiet residential neighborhoods with a semi-rural lifestyle characterize the community.

General Plan

The City's General Plan is a dynamic document consisting of several elements: Framework, Conservation, Housing, Open Space, Service Systems, Transportation, Air, Noise, Safety, and Land Use.

The Land Use Element of the General Plan is comprised of 35 Community Plans that are the official guide to the future development of the City of Los Angeles. The Community Plans are intended to promote an arrangement of land uses, streets, and services which encourage and contribute to the economic, social, and physical health, safety, welfare, and convenience of the people who live and work in the community. The Community Plans ensure that sufficient land is designated for housing, commercial, employment, educational, recreational, cultural, social and aesthetic needs of the City's residents.

General Plan Land Use

The 35 Community Plans of the City of Los Angeles, which constitute the Land Use Element of the General Plan, guide the location and intensity of private and public uses of land; direct the arrangement of land uses, streets, and services; and encourage the economic, social, and physical health, safety, welfare, and convenience of people who live and work in the community. Land uses are organized into general classifications — residential, commercial, industrial, and open space — which are further defined by use, intensity, and density as noted on Table 3-1, Sylmar Community Plan Land Use. Each land use category includes a list of permitted zones, which delineate the types of uses, densities, intensities, and building heights permitted on a particular parcel. Figure 3-1, Sylmar General Plan Land Use Map, is a graphic representation of the location of Sylmar's land use classifications that reflects the policies contained in the Community Plan.



The Sylmar Community Plan designates areas appropriate for housing growth, such as those near transit options.

TABLE 3-1
General Plan Land Use*

Land Use Designations	Corresponding Zones	Net Acres	Total Acres	% of Total Acres
Total			6,808	
Residential			4,271	63%
<i>Single Family Neighborhoods</i>			3,850	57%
Minimum	A1, A2, RE40	770		11%
Very Low I	RE20, RA	790		12%
Very Low II	RE15, RE11	251		4%
Low I	RE9, RS	870		13%
Low II	R1	913		13%
Low III	RD5, RD6	256		4%
<i>Multiple Family Neighborhoods</i>			421	6%
Low Medium I	RD4, RD3	258		4%
Low Medium II	RD1.5, RD2	77		1%
Medium	R3	86		1%
Commercial			213	3%
Neighborhood Commercial	CR, C1, C1.5	21		<1%
Community Commercial	C2, C4, R3, RAS3	192		3%
Industrial			584	9%
Limited Industrial	M1, MR1	479		7%
Light Industrial	M2, MR2	105		2%
Other			1,740	26%
Open Space	OS, A1	678		10%
Public Facilities	PF	889		13%
Public Facilities - Freeways	PF	173		3%

* Updated on 6/5/2015

Angeles National Forest

San Gabriel Mountains

Sunland - Tujunga - Lake View Terrace - Shadow Hills - East La Tuna Canyon

City of San Fernando

Mission Hills - Panorama City - North Hills

Granada Hills - Knollwood

Figure 3-1 General Plan Land Use Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

City of Los Angeles Planning Department • May 2015

Not to Scale

Legend:

Residential

Single Family Neighborhoods

- Minimum
- Very Low I
- Very Low II
- Low I
- Low II
- Low III

Multiple Family Neighborhoods

- Low Medium I
- Low Medium II
- Medium

Commercial

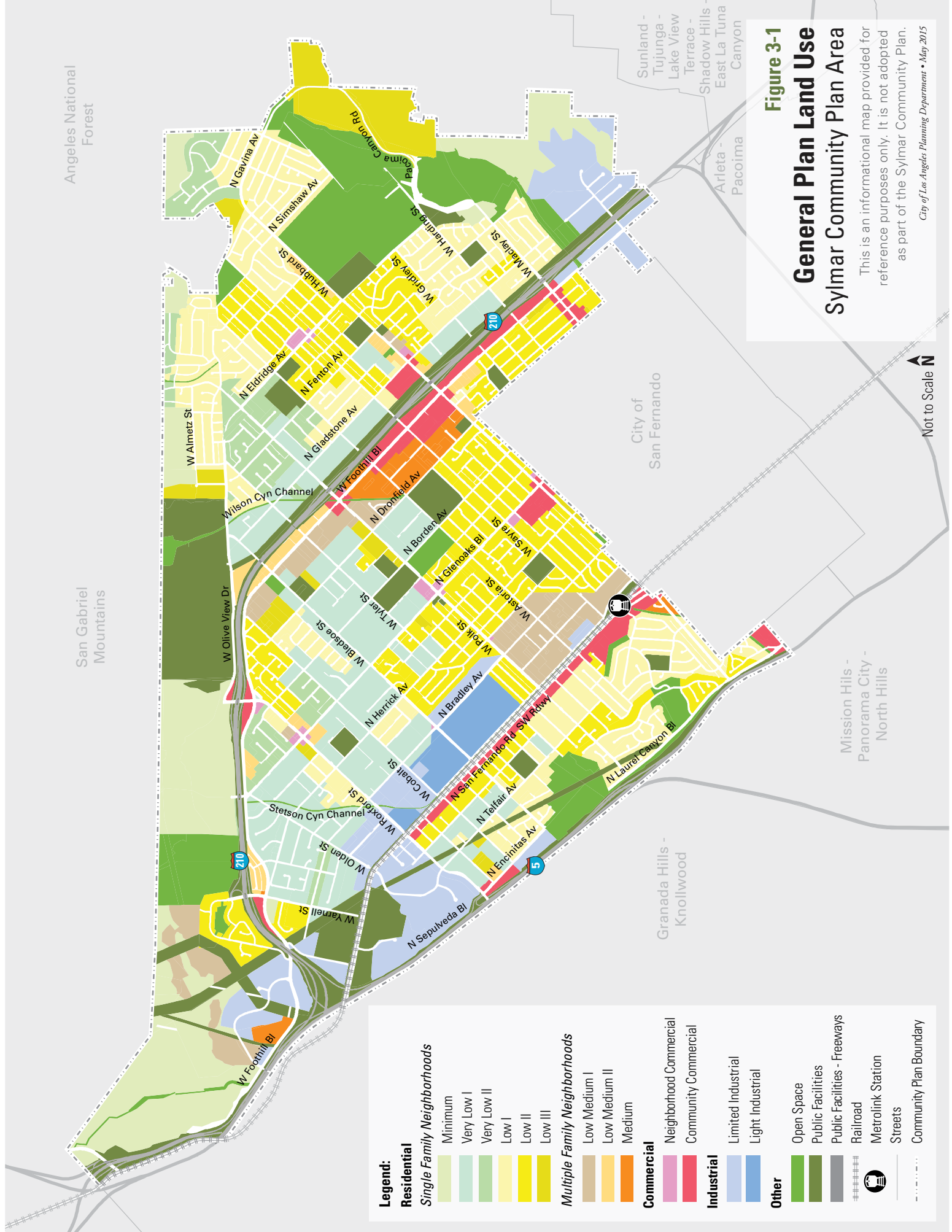
- Neighborhood Commercial
- Community Commercial

Industrial

- Limited Industrial
- Light Industrial

Other

- Open Space
- Public Facilities
- Public Facilities - Freeways
- Railroad
- Metrolink Station
- Streets
- Community Plan Boundary



Community Themes

As discussed in Chapter 1, the General Plan Framework Element establishes guiding principles for growth and development for the city. While all Community Plans implement these guiding principles, Los Angeles is a city of diverse neighborhoods and communities and with that comes many varied and localized issues. The Sylmar Community Plan carries out the Framework guiding principles through its land use designations, its policies and specific community focused themes. The community themes provide more detailed expression of the community's vision statement and lay the foundation for the Community Plan's goals, policies, design guidelines, and implementation programs. They build on major points of agreement that emerged from community discussions about the valued qualities of Sylmar, hopes and aspirations for the future, and strategies for achieving the vision. The Sylmar Community Plan Themes are as follows:

Preserve the Semi-Rural Suburban Character.

Preservation of rural character has been an ongoing theme for many communities in the North Valley. As development pressures increase, the challenge of preserving the unique rural character of each community also increases. Residents of Sylmar enjoy a semi-rural suburban lifestyle and want to recognize and protect the qualities that contribute to this way of life. Preserving the community's semi-rural character also protects the environment from adverse impacts associated with more intense development. The abundance of natural features in Sylmar, such as the "sea of trees", contributes to the high quality of life and the semi-rural character that attracted residents to the community in the first place. Where development of land is proposed, the densities and design should be of a nature that will maintain the semi-rural character of the area. This Community Plan preserves the character of existing single-family and equestrian-oriented neighborhoods which promote Sylmar's agricultural heritage by maintaining lower density land use designations, limiting the allowed residential density of some commercial areas, and establishing goals and policies that support the development of an equestrian trails system.

Protect the Environment and Natural Resources and Expand Recreational Amenities and Opportunities.

Promoting a healthy environment is critical to maintaining a sustainable community. Sensitive areas such as the Pacoima Wash and the watershed, the upper portions of the community adjacent to the Angeles National Forest, and other valuable resources and recreational amenities should be protected and expanded. While new development promotes economic vitality, it should not adversely affect the community's environmental quality and access to public open spaces and recreational opportunities. With new development, there is an opportunity to implement more environmentally-friendly building practices such as installing permeable surfaces, planting fire resistant landscapes, and integrating the trails network. This Community Plan contains goals and policies that help to protect and restore the environment and improve the quality of life for residents, now and in the future.

Sustainable Development

Sustainable development encompasses established principles of good planning and advocates a proactive approach to future development. The basic concept of sustainability is meeting the needs of current generations without compromising the ability of future generations to meet their own needs. Sustainable development can be further defined as promoting the "three E's": environment, economy, and equity.

What does sustainable development look like on the ground? In a community that is developing sustainably, the neighborhood is the basic building block of urban design and is characterized by walkability, mixed-use development, and mixed-income housing. Walkability is a function of compactness and density. Attention to streetscape and public spaces is a key design element in creating desirable places to live. Such neighborhoods are more likely to support efficient transit systems. The character and function of each neighborhood is then placed properly within its regional setting. This approach to planning, from the neighborhood to the regional level, is often referred to as "smart growth."

Expand Housing Opportunities.

This Community Plan creates new housing options, mostly near the Sylmar/San Fernando Metrolink Station and in areas identified for mixed-use, in accordance with the Framework guiding policy to focus growth in higher-intensity commercial centers close to transportation and services. Many of the community's neighborhoods, including single-family and equestrian-oriented neighborhoods, are established and not expected to change significantly as growth in other parts of the community occurs. This Community seeks to direct growth away from established residential neighborhoods towards transit-oriented districts and corridors near commercial centers. Other areas – particularly near the Metrolink Station – will support an increase in residential and commercial activity.

Enhance Distinct Neighborhoods, Districts, Centers, and Mixed-Use Areas.

The establishment of lively and walkable commercial districts is a key goal of this Community Plan to retain elements of Sylmar's small town environment. Community input received during the outreach process reflected a strong need for shopping and other community services within walking distance of neighborhoods. This Community Plan aims to support the creation and maintenance of distinctive neighborhoods, districts, and centers that provide visual diversity, varying intensities of residential and commercial activity that are appropriate to their location, and plentiful opportunities for social interaction. It identifies districts, centers, and boulevards that reflect a defined local character, scale, and relationship to adjacent neighborhoods and provide a full complement of uses with easy access to parks, stores, and other amenities of everyday living. Development intensities are designed to retain low-scale residential neighborhoods, concentrate more intense development in specific locations, and maximize accessibility to amenities, while providing transition in scale and height to lower-density neighborhoods.



Pedestrian amenities, visually intriguing landscaping, and areas where people can interact help create distinct and livable neighborhoods.

Promote the Metrolink Station as the Town Center of Sylmar.

Identified as the “heart” of the community, the area immediately surrounding the Sylmar/San Fernando Metrolink Station is considered a transit-oriented town center with substantial development opportunity. In particular, San Fernando Road has a significant amount of underutilized commercial land and has the potential for much greater land use intensity which could accommodate a variety of services and uses. A mix of housing, retail, service, and civic/institutional amenities that are well-designed and inviting for people to walk and ride their bikes are encouraged and supported along this corridor by the new Community Plan. Office space and other employment-generating uses which provide employment opportunities and help support the existing local economy are also encouraged in the new Community Plan.

Preserve Industrial Areas for Local Jobs and Revenue.

Industrial uses, particularly specialty firms that are solely allowed in industrial zones, are disappearing. Industrial districts are increasingly compromised by conversion pressures into commercial and residential uses. A renewed commitment to the preservation of industrial zones for jobs in Sylmar can improve the jobs-housing balance, maintain a healthy local economy with revenue and employment opportunities, help pay for services such as fire, police, and libraries, and help ensure appropriately located land suitable to accommodate existing, new and relocating industrial firms. The various industrial districts in Sylmar are important economic and employment areas serving the local community and the greater San Fernando Valley. This Community Plan retains the industrial land use designation and zoning in Sylmar and contains goals and policies that help to support existing businesses and encourage new business opportunities that are compatible with the environment.



Preserving industrial land in Sylmar helps ensure local employment opportunities and economic growth.



The Sylmar/San Fernando Metrolink Station provides access to multiple transit opportunities, increasing access to jobs and amenities and decreasing vehicle dependence for those who live nearby.

Residential Areas

Most of Sylmar is designated for residential uses, with single-family residential as the predominant land use, as seen on Figure 3-2, Residential Areas. Single-family neighborhoods are located throughout the community, with equinekeeping neighborhoods clustered primarily in the northern and central portions of the community. Multi-family neighborhoods are concentrated in the central portions of the community, along major corridors.

The community of Sylmar contains a wide range of housing options, meeting the various needs of a diverse population. Residential types vary from large equinekeeping lots with horse stables and corrals, to compact single-family dwellings with panoramic views of the foothills, to manufactured homes, and rental and ownership multi-family units. The housing stock also varies in age, from homes built over a century ago to new townhomes built this year.

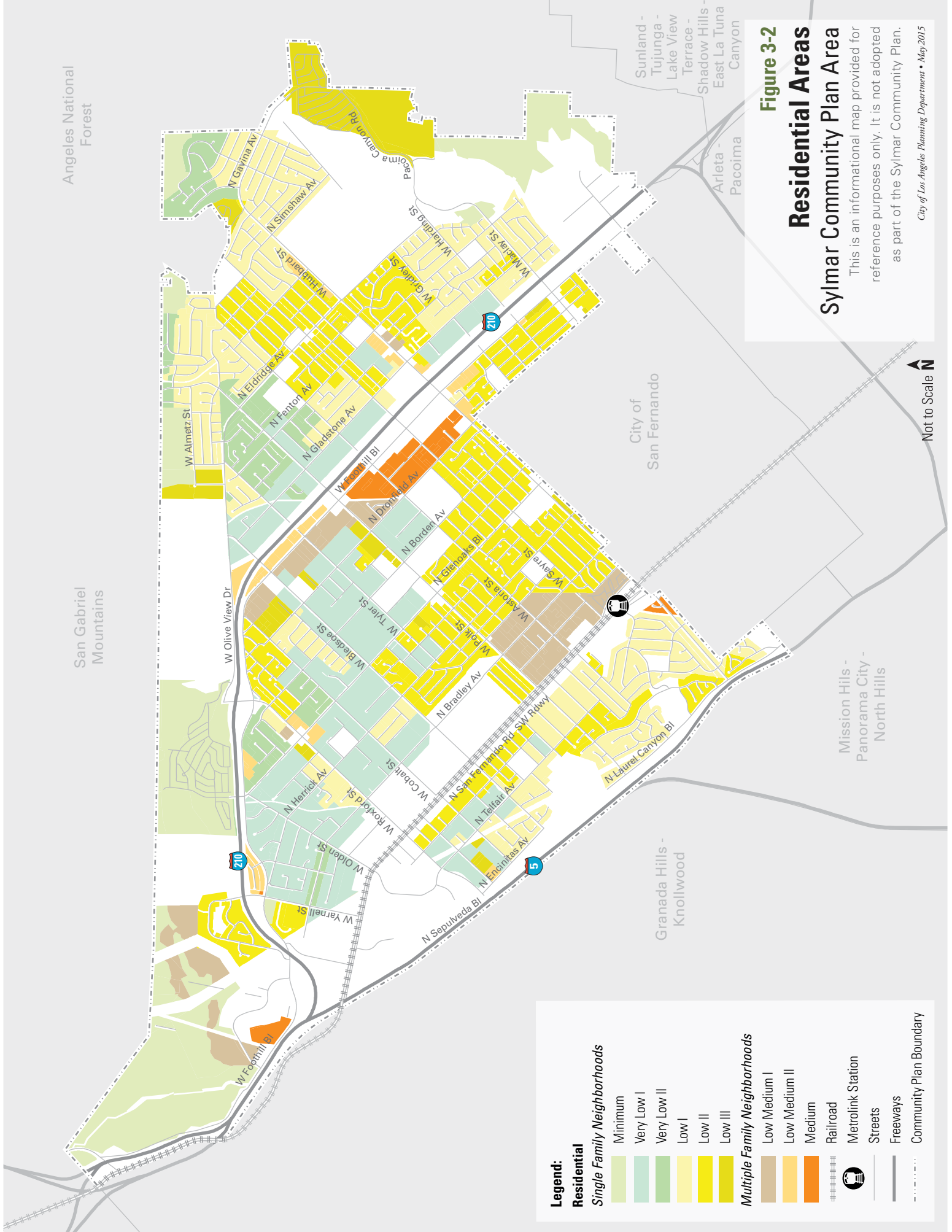
Sylmar will remain a predominantly semi-rural, suburban residential community. Most of the residential neighborhoods are well established and not expected to change significantly. Growth will be directed to major corridors with public transit, mixed-use boulevards, and existing neighborhoods in transition to higher density.



Ranch style architecture is typical of residential neighborhoods in Sylmar, representing the semi-rural character of the community.



Single family residential homes vary in age and type, with more recently constructed subdivisions compatible with surrounding neighborhoods.



Angeles National Forest

San Gabriel Mountains

City of San Fernando

Granada Hills - Knollwood

Mission Hills - Panorama City - North Hills

Sunland - Tujunga - Lake View Terrace - Shadow Hills - East La Tuna Canyon

Figure 3-2
Residential Areas
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

City of Los Angeles Planning Department • May 2015

Not to Scale

Legend:

Residential

Single Family Neighborhoods

- Minimum
- Very Low I
- Very Low II
- Low I
- Low II
- Low III

Multiple Family Neighborhoods

- Low Medium I
- Low Medium II
- Medium

Railroad
 Metrolink Station
 Streets
 Freeways
 Community Plan Boundary



In maintaining single family neighborhoods, the community plan preserves the equestrian lifestyle and the overall semi-rural character of the community.



With new residential development, there is an opportunity to construct new trails or improve existing trails, which will improve trail safety and connectivity throughout the community and region.

Residential Issues and Opportunities

Throughout the Sylmar Community Plan update process, opportunities were provided for community members and stakeholders to comment on the proposed plan changes at a variety of public workshops and presentations. As a result of public input and staff research, issues and opportunity areas were identified. Goals and policies were then developed to specifically address these areas, and are included in this section. A primary concern was the preservation of neighborhood character and the need for neighborhood services within walking distance. Following is a summary of key issues and opportunities:

- Residents and visitors alike have had a longstanding concern for preserving the equestrian lifestyle and amenities in this area. As development pressures have mounted in these lower-density portions of the City, equestrian-oriented lots have been subdivided into sizes too small to accommodate equines, buffers between equine and non-equine lots have been diminished, and commonly used trails have also been obscured or interrupted by new developments. This pressure has resulted in a loss of valuable and irreplaceable equinekeeping lots that contribute to the rich rural character of the area. Improved regulations can assure existing equestrian-oriented neighborhoods are preserved and that future development in these areas contributes to the community's overall semi-rural way of living. Amending the Equinekeeping "K" District with requirements to preserve minimum lot sizes and features of equestrian lots, as well as an interconnected trails system in the area, will ensure preservation of the equestrian lifestyle in Sylmar and adequate buffering between uses.
- Development pressures over time have increased density in areas that have been predominantly low-density residential. Preserving lower-density areas with land use designation and zoning will help to retain the larger lot sizes and semi-rural suburban character of this area. It will also direct new higher density growth where it is more appropriate, such as the areas near the Sylmar/San Fernando Metrolink Station and commercial areas and mixed-use boulevards along major corridors. This will provide new housing opportunities that are connected to public transit, services, and facilities which incentivize walking and biking and reduce vehicle trips and miles traveled.
- There is an interest in environmental protection, in particular preserving wildlife corridors from housing developments that interfere with wildlife passage and maintaining unobstructed mountain views. Minimizing and regulating development along the Foothills and the Pacoima Wash through zoning, policy and design guidelines will help preserve wildlife corridors and scenic vistas.

- Public safety with regard to natural disasters, protection from wildfires, mudslides, and flooding are among some the issues of particular concern to residents. Retaining lower densities near the foothills and minimizing development will help to provide for defensible spaces against disasters, and allow percolation of rain and run-off water into the Tujunga/Pacoima Watershed, recharging the City’s groundwater.
- Inconsistent zoning and development patterns over several decades have created inappropriate or non-existent transitions in use, scale and massing from multi-family areas and commercial boulevards to adjacent residential and equinekeeping neighborhoods. Improved transitions and connectivity between various types of residential, commercial and industrial land uses will create better development and neighborhoods in the future.
- The need for affordable senior housing and assisted living facilities is a key concern due to demographic and economic trends and projections. In Sylmar, such facilities would increase the opportunities for “empty nest seniors” to downsize from large single-family homes while remaining within the community and the reach of supportive social, cultural and family networks.
- Preserving small neighborhood serving amenities within residential areas serves the larger goal of reducing vehicle trips by making walking or bicycling more viable options for simple conveniences.



Residential neighborhoods consisting of manufactured homes offer alternative, affordable, single-family housing options for residents of the community.



Senior housing and assisted living facilities provide local housing options for the growing population of seniors.



Residential neighborhoods in Sylmar consist largely of single-family homes that boast large landscaped setbacks and mature shade trees along the public right-of-way.



This pocket park provides opportunities for open space, playground and picnic areas, trails, and walkways and offer a significant option for regular physical activity for residents.

All Residential Areas

The goals and policies in this section reflect the community's vision to preserve the low-density, single-family, equestrian-oriented neighborhoods that provide quality residential environments, while providing new housing opportunities that enhance existing residential neighborhoods, complement the surrounding environment, and provide residential amenities. Each of the community-specific design guidelines in this section and the Residential Citywide Design Guidelines should be considered, although not all will be appropriate in every case, as each project will require a unique approach. However, to achieve the goal of excellence in new design, projects should incorporate these design guidelines to the maximum extent feasible, in order to meet the intent of this Community Plan.

Goal LU1: Complete, livable and quality neighborhoods throughout Sylmar that provide a variety of housing types, densities, forms and design, and a mix of uses and services that support the needs of residents.

Policies

- LU1.1 **Choice in Type, Quality, and Location of Housing.** Provide a variety of housing types that accommodates households of all sizes and for all persons regardless of income, age, ethnic background, and physical needs throughout Sylmar's residential neighborhoods and in targeted mixed-use areas.
- LU1.2 **Existing Housing Stock.** Minimize the loss of good quality, affordable housing and encourage the replacement of demolished housing stock with new affordable housing opportunities. Minimize displacement of residents when building new housing.
- LU1.3 **Recreational Amenities.** Incorporate amenities for residents, such as on-site recreational facilities, passive open spaces, and community gardens which promote physical activity, fitness, and health.
- LU1.4 **Land Lease Communities.** Preserve existing and support new land lease communities, such as mobile home parks and manufactured housing. Encourage the rehabilitation and modernization of aging and substandard land lease communities, while addressing the displacement of residents.
- LU1.5 **Affordability.** Promote the use of existing citywide programs to increase rental and housing ownership opportunities, such as small lot subdivisions, when and where appropriate, and density bonuses in exchange for affordable housing set asides.
- LU1.6 **Multiple Housing.** Direct multi-family housing growth to neighborhoods designated as Low Medium and Medium Residential.

Goal LU2: **Well-designed residential neighborhoods that enhance the pedestrian experience and exhibit the architectural characteristics and qualities that distinguish Sylmar.**

Policies

- LU2.1 **High-Quality Development.** Design projects to achieve a high level of quality in accordance with the Sylmar Community Plan Design Guidelines for Residential Areas, Residential Citywide Design Guidelines, and other applicable design guidelines. Projects are required to incorporate to the maximum extent feasible applicable design guidelines.
- LU2.2 **Neighborhood Compatibility.** Require infill development, new structures and additions to be compatible with the suburban characteristics and qualities of existing residential neighborhoods and dwelling units with regards to scale, mass, building orientation, heights, setbacks, and entrances, topography, parking arrangement, landscaping, and parkways.
- LU2.3 **Design Standards.** Support design standards to achieve transition in scale when neighborhoods planned for multifamily residential uses abut existing single family residential uses and/or neighborhoods planned for single family residential uses.
- LU2.4 **Utility Design.** Integrate service elements and infrastructure such as mechanical equipment, trash enclosures and utilities with the design of projects. Locate service elements and infrastructure away from street views and screen and/or enclose equipment in order to enhance the pedestrian experience and aesthetic appeal of the building and overall neighborhood. Underground utilities where possible.



Parking is located to the rear of the building and away from the corner. Access is taken from a side street.



At-grade, open and inviting front yards help activate the street, maintain human scale, and soften the transition of buildings to the street. The low wall also encloses private open space while still allowing social interaction.

Goal LU3: **Residential neighborhoods that embrace and contribute to the semi-rural, equestrian lifestyle of the surrounding area and preserve the rustic charm enjoyed by residents.**

Policies

- LU3.1 **Historic Resources and Significant Features.** Preserve existing historic resources, significant vegetation, trees, and other natural features which contribute to the overall charm and rural character of the area. Encourage the rehabilitation and rebuilding of deteriorated housing as a means of preserving Sylmar’s character.
- LU3.2 **Character.** Design residential development, especially development adjacent to equinekeeping lots, to contribute to the equestrian and rural lifestyle of the area by incorporating defining characteristics such as ranch-style and western architecture and landscaped open spaces.

Trails

Trails throughout Los Angeles take different forms and are implemented in a variety of ways. This Community Plan indicates on the Trail Network map, as shown in Figure 4-5, both the Rim of the Valley Trail Network, which consists of existing and proposed trails, as well as other existing and proposed trails. This map serves as a tool for improving trail awareness, quality, safety, and connectivity.

Some trails are located on public land in parks, or on dirt trails through vacant land, while others are located in the street alongside vehicles and bicycles and are not always marked. Trails alongside streets or traversing parks may be either informal dirt tread, or improved tread with rails or fences. The trails alongside the streets may be within the public right-of-way, or within easements on private property. Some trails alongside roads consists of maximized space in the public right-of-way, resulting in one trail that has adequate width to accommodate horses and/or other trail users. Trails through undeveloped land may be informal, traditionally used trails, or easements, which sometimes have established right of public access. As part of discretionary actions on division of land and other cases, decision-makers may require improvement of an existing trail with tread and fences or rails. The City may also sometimes require dedication of an easement to the public right-of-way, Chapters 3, 4, and 5 contain goals, policies, and design guidelines for trails. In addition, the Sylmar Equinekeeping “K” District contains conditions, in addition to the Los Angeles Municipal Code requirements, which support equinekeeping on residential lots and the Trail Network.

LU3.3 **Trail Network and Connections.** Reinforce the viability of equine uses and accessibility to open space and recreational opportunities by requiring new developments, subdivision tracts, parcel maps, small lot subdivisions, and infrastructure improvement projects that abut or connect with a trail to develop and/or improve the Trail Network, as shown on Figure 4-5. A formal dedication for a public easement for trail purposes shall be required. Refer to Chapter 4 for additional policies and design guidelines.

LU3.4 **Non-Public Trails.** New developments, subdivision tracts, parcel maps, and small lot subdivisions over which a trail is shown on Figure 4-5, Trail Network, may require dedication where a prescriptive right of access has been established. Projects may offer a private easement to grant public access to trails, if in accordance with the applicable Sylmar Design Guidelines.

Goal LU4: **Distinct equestrian-oriented neighborhoods that promote and enhance Sylmar’s agricultural and equestrian heritage, semi-rural character, and horsekeeping way of life.**

Policies

LU4.1 **Preservation of Equinekeeping Areas.** Strongly discourage discretionary actions, including zone changes, zone variances, conditional use, or divisions of land, that do not preserve equinekeeping and agricultural uses within Sylmar’s equinekeeping districts and neighborhoods.

LU4.2 **Equinekeeping Lot and Use Preservation.** Maintain a minimum lot size of 17,500 square-feet in designated residential areas that are within an Equinekeeping “K” District.



When new residential development is located adjacent to an identified trail, trail development and improvement as part of the project will provide linear open space and additional recreational opportunities for residents.



Existing neighborhoods are encouraged to develop or improve trails, which will enhance equestrian lifestyle.

- LU4.3 **Agricultural Lot and Open Space Preservation.** Maintain a minimum of one acre in all designated Minimum residential areas, especially adjacent to the foothills, the Pacoima Wash, and other open space areas and natural resources.
- LU4.4 **Legal Non-Conforming Equinekeeping Lot Protection.** Maintain existing legal non-conforming equinekeeping lots and uses, regardless of whether they are located within an Equinekeeping "K" District, to preserve the equestrian heritage within Sylmar.
- LU4.5 **Expansion of Equinekeeping Districts.** Establish new, or expand existing, equinekeeping districts where appropriate and feasible.

Goal LU5: **Lots within and adjacent to equinekeeping areas that are designed to ensure the feasibility of equinekeeping on equestrian lots and compatibility with adjacent non-equinekeeping lots.**

- LU5.1 **Division of Land and Grading.** Design new subdivision tracts, parcel maps, and small lot subdivisions within an Equinekeeping "K" District according to the Sylmar Design Guidelines to ensure room for on-site horse facilities and site accessibility via an equine path from the driveway to the pad area. Achieve the intended purpose of the "K" District by providing adequate and level equine pad areas, and arrange the location of homes, equine pads and stables, lot access and gates, trails and trail easements and access points, and grading, according to the Design Guidelines.
- LU5.2 **Transitions.** Provide additional setbacks and density fading on all higher density development, including all residential and non-residential development, adjacent to equinekeeping lots regardless of location within an Equinekeeping "K" District.
- LU5.3 **Additional Structures.** Second dwelling units or accessory living quarters are permitted only as permitted by LAMC and if they do not adversely affect any equinekeeping uses on the subject lot or adjacent lots, or the ability to maintain equinekeeping uses on the subject lot or any adjacent lot. Restrict permanent structures, including swimming pools and tennis courts, from being constructed or located within any portion of the required equine pad and stable areas.
- LU5.4 **Proper Distances from Stables/Stalls.** Maintain proper distances from equine stables/stalls and habitable rooms of any dwelling unit and neighbor's dwelling unit, including accessory living quarters. Refer to the Sylmar Design Guidelines.
- LU5.5 **Notification of Future Residents.** Approval of non-equinekeeping developments adjacent to equinekeeping lots should include notification to future residents that animal keeping uses are permitted in adjacent lots.

Goal LU6: Safe, well designed hillside development that complements the natural environment and preserves the beautiful scenic vistas, foothills, and vast open spaces.

- LU6.1 **Hillside Density.** Limit the intensity and density in hillside areas to that which can be reasonably accommodated by infrastructure and natural topography. Notwithstanding any land use designation maps to the contrary, all projects with average natural slopes in excess of 15 percent, including Tract Maps and Parcel Maps, shall be limited to the minimum density housing category for the purposes of enforcing the slope density formula of LAMC Sections 17.05C and 17.50E.
- LU6.2 **Emergency Access.** Design developments to provide for safe, direct, and efficient exit of residents during emergency events. Investigate and consider feasible secondary access connections as part of hillside subdivisions. Require extensions, completions, and connections of existing street network where feasible to provide secondary access to hillside development.
- LU6.3 **Slope Preservation and Grading.** Cluster homes on those portions of undeveloped hillside areas that have less than a 15 percent slope in order to retain the steeper slopes in their natural state or in a natural park-like setting, minimize the amount of grading and the alteration of the natural topography, and provide more open space opportunities for recreation and equestrian use. The density pattern indicated in the Community Plan may be adjusted to facilitate development on the more level portions of the terrain provided that the total number of dwelling units indicated in any development is not increased over that allowed by the Community Plan based on the net area of development.



This hillside subdivision tract provides open landscaped setback areas and pedestrian sidewalks. Trees are located in the front setback area in lieu of a parkway between the sidewalk and the street.



Hillside development should provide safe alternative routes and walking paths for hillside residents in case of an emergency or natural disaster.

Ridgelines

Ridgelines are characterized as being prominent backdrops where development should not occur. The San Gabriel Mountains, located on the northern edge of the Community Plan, is an example of a significant ridgeline that should be protected.

- LU6.4 **Topography Preservation.** Use the natural topography as the primary criteria to determine the placement and/or alignment of houses, roads, drainage facilities, equestrian facilities, and other necessary structures. Design developments to be integrated with and visually subordinate to natural features and terrain. Condition new development in the hills to protect public views from public roadways and parklands to the maximum extent feasible.
- LU6.5 **Mountain Viewshed Protection.** Design development near ridgelines so that it does not break the mountain silhouette of a significant ridgeline. Discourage building and grading on ridgelines to protect ridges and environmentally sensitive areas, and to prevent erosion associated with development and visual interruption of the ridge profile.
- LU6.6 **Development Limitations.** Support hillside mansionization regulations to protect single-family neighborhoods from out-of-scale development throughout Sylmar. Enforce the City's Baseline Hillside Mansionization Ordinance.
- LU6.7 **Retaining Walls.** Minimize the use of retaining walls and when necessary design them to be compatible with the architectural style, materials, and detail of the principal structure. Utilize stepped or terraced retaining walls with planting or trails where appropriate, as an alternative to tall retaining walls. Consider living walls systems, or garden on walls, as an alternative to concrete where retaining walls are necessary.
- LU6.8 **Landscaping.** Incorporate landscaping that supports slope stability and provides fire protection.



Constructing single-family homes below significant mountain ridgelines will preserve the scenic views of the hillsides in Sylmar.



Siting and construction of structures with regard for the natural topography helps prevent soil erosion while preserving mountain views along the foothills.

Goal LU7: **Environmentally sustainable residential development that uses green design and technology and water conservation methods to minimize consumption of non-renewable natural resources and replenish the City’s watershed by capturing and storing water in Sylmar’s underground basin.**

- LU7.1 **“Green” Building.** Utilize “green” building strategies such as solar panels, insulating buildings to minimize consumption of non-renewable natural resources, and orienting windows, building volumes and second stories to maximize solar access.
- LU7.2 **Permeable Surfaces.** Increase areas of permeability by minimizing driveway and curb cut widths, limiting driveway paving to the width required to access a garage, and utilizing permeable surfaces on driveways, walkways, trails, and outdoor spaces in order to capture, infiltrate, and store water underground.
- LU7.3 **Landscaping.** Retain existing vegetation and trees and use native and drought-tolerant landscape and drip irrigation when developing the site in order to conserve water.
- LU7.4 **Canopy Trees.** Provide canopy trees in planting areas for shade and energy efficiency, especially on south and southwest facing facades.



Residential solar panels promote environmentally-friendly power, using renewable energy from the sun. Solar panels can add value to a project when integrated appropriately.



Bioswales in the streetscape design of a neighborhood can increase storm water infiltration and remove pollution and silt from surface runoff water.

Single-Family Residential Neighborhoods

The community of Sylmar consists of mostly single-family residential neighborhoods built post World War II with modest ranch style homes, as seen on Figure 3-3, Single-Family Residential Areas. These neighborhoods boast large mature trees and spectacular views of the foothills that lie within the Tujunga/Pacoima Watershed. The neighborhoods in the north central areas of the community are among the few places in the city where one can maintain equine and other limited livestock and get a glimpse of the City's agricultural and equestrian heritage. These neighborhoods and surrounding areas feature semi-rural characteristics such as large equinekeeping lots with access paths for horse trailers, horsekeeping areas with stables and corrals, and equestrian trails that lead to the Angeles National Forest and the Rim of the Valley Trails Corridor. The neighborhoods in the southern portion contain some of the oldest and most affordable housing stock in the community, featuring cozy single-story homes built on traditionally small lots. More recent neighborhoods can be found along the fringes of the community, consisting of two-story suburban townhomes and manufactured housing.

Sylmar's single-family residential neighborhoods are an important element of the community's character and identity. Most of the single-family residential neighborhoods are not expected to change significantly. The majority of the additional housing units would be accommodated in areas in transition, which include areas along Roxford, Polk, Hubbard, Gladstone Avenue, and Foothill Boulevard. The goals, policies, and design guidelines in this Community Plan reflect the community's vision to preserve its single-family and equestrian neighborhoods while providing additional housing opportunities that embrace the rural charm, character, and lifestyles of the area.



The single family neighborhood are among the most prominent land uses in Sylmar.



The single story-shaped ranch style home is characteristic of Sylmar.

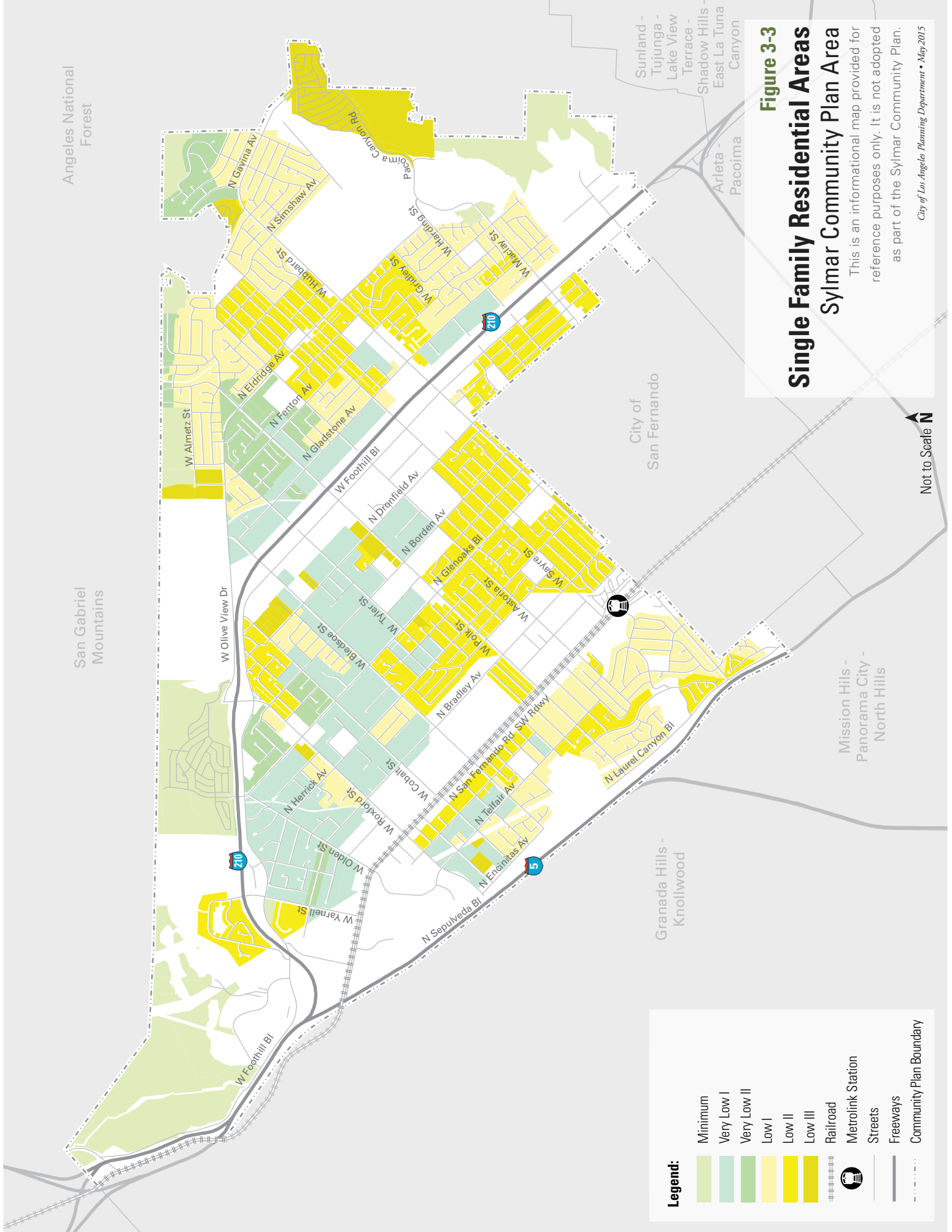


Figure 3-3
Single Family Residential Areas
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Legend:

- Minimum
- Very Low I
- Very Low II
- Low I
- Low II
- Low III
- Railroad
- Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Not to Scale **N**

Goal LU8: Established low-density single-family neighborhoods that provide quality living environments for a diversity of households and new housing opportunities that enhance the existing low-density residential and semi-rural, suburban lifestyle that has long characterized the community of Sylmar.

- LU8.1 **Neighborhood Preservation.** Preserve single-family zoned residential neighborhoods, while maintaining the existing character and scale. Enforce the City's Baseline Mansionization ordinance.
- LU8.2 **Housing Density.** Maintain the existing density of single-family residential neighborhoods by directing intensive residential development to areas identified in this Community Plan that have the capacity to accommodate such growth, to neighborhoods designated as Low Medium and Medium Residential.
- LU8.3 **Flag Lots.** Discourage flag lots as part of any subdivision tract, parcel map, or small lot subdivision. Allow flag lots only when property development is not otherwise practical due to adjacent topography and/or when street access cannot reasonably serve the lot and all other alternatives have been exhausted, and with application of design guidelines.

Goal LU9: New subdivision tracts, parcel maps, or small lot subdivisions which are compatible with the environment and surrounding development pattern and overall neighborhood character with respect to density, lot size and width, grading, setbacks, orientation, streetscape, and circulation.

Low Density for Smart Growth

An important principle of "smart growth" is preserving open space, farmland, natural beauty, and important environmental areas and redirecting population growth to areas with more public transit and employment options through increased housing density in areas where transit and job centers are located. Retaining the low density development and agricultural designations and zones in Sylmar helps direct more intensive development, and therefore more of the population, to the public transit and job centers of Los Angeles. Practicing smart growth in this way supports a more environmentally-sustainable and efficient city, as well as a variety of housing options throughout the city as a whole.

- LU9.1 **Lot Size and Width.** Transition new development with regards to lot size and width so that new lots are compatible with existing adjacent lots and surrounding neighbors. Lots may be increased in size so as to more closely conform to the size of existing contiguous lots or nearby parcels.
- LU9.2 **Setbacks.** Where there is an existing average prevailing setback, apply the setback to provide continuity along the street edge. Apply additional setbacks in side and rear yards abutting equinekeeping lots.
- LU9.3 **Grading.** Work with the natural topography of the site to avoid dramatic and unnecessary grade changes by utilizing landform grading.
- LU9.4 **Building Orientation.** Maintain the suburban character of the neighborhood by configuring buildings to front public streets, not driveways. Design corner buildings to be prominent by building to both the front and side property lines facing a street.
- LU9.5 **Garage Location and Driveways.** Consider prevailing garage location and driveway patterns by incorporating such patterns into the design scheme. Innovative designs are encouraged that minimize the visual impact of garages facing the street.
- LU9.6 **Streetscape.** Continue to provide open and inviting landscaped yard setbacks and parkways by restricting gated or walled communities that isolate the project from the neighborhood and surrounding community.



Landscaped low-lying walls and fences, as shown in this subdivision tract, are characteristic of Sylmar's residential neighborhoods.



Landscaped front yard setbacks and parkways are common in many Sylmar neighborhoods and represent the suburban character of the community, as shown in this example.

- LU9.7 **Fences and Walls.** Utilize low-lying, open fence design and limit walls and fences within the street-side setback areas in order to provide front entries that are visible from the street. Attractively landscape the setback areas to screen and enhance their appearance.
- LU9.8 **Road Width.** Consider existing road widths and streetscape patterns to avoid unnecessary non-contiguous improvements of sidewalks, curbs, and streets



Landscaped front yard setbacks and parkways are common in many Sylmar neighborhoods and represent the suburban character of the community, as shown in this example.



Landscaped setbacks contribute to neighborhood aesthetics and pleasant pedestrian environments.

Multiple Family Residential Neighborhoods

Multiple family residential neighborhoods are predominantly located along Foothill Boulevard in the east central portion of Sylmar with varying types of housing units interspersed throughout the community, as shown in Figure 3-4, Multiple Family Residential Areas. The northern portion along Foothill Boulevard consists of smaller scale two-story condominiums and apartments that are characterized by private pedestrian entrances and mature street trees near equestrian-oriented neighborhoods while the southern portion ranges from two to four-story buildings with varying streetscape patterns. Additional housing units are located in the western portion, near the Sylmar/San Fernando Road Metrolink Station, and are among the oldest in the community, consisting of variety of apartment units and mobile home parks that cater to senior citizens.

Sylmar’s multi-family residential neighborhoods offer a variety of ownership and rental opportunities that are near public transit, services, and amenities, meeting the various needs of a diverse population. The majority of the additional housing units would be accommodated along Foothill Boulevard, Maclay Street, San Fernando Road, and around the Metrolink Station. The goals, policies, and design guidelines in this Community Plan aim to strengthen existing multiple family residential neighborhoods while ensuring that new development provides additional amenities, well-designed and compatible with its environment.



Multiple family housing that incorporates building articulation, landscaping, and entryways that face the street complement nearby single family neighborhoods and provide more attractive homes.



Directing pedestrian access to the street encourages community members to walk when development is at grade separation.

Goal LU10: A variety of multiple family housing located on or near major corridors that provide safe and convenient access to public transit, services, and amenities.

LU10.1 Corridor Development. Improve and stabilize existing multiple family residential neighborhoods, allowing for growth particularly in areas along Foothill Boulevard and adjacent streets – Maclay Street, San Fernando Road, and within a half mile of the Sylmar/San Fernando Metrolink station – where there is sufficient public infrastructure and services.

LU10.2 Pedestrian Pathways. Provide accessible, comfortable, and safe pedestrian connections within and around the project. For units not adjacent to a street, provide landscaped pedestrian pathways that are separate from auto circulation routes.

LU10.3 Entrances. Promote pedestrian activity by placing entrances at grade level or slightly above, and unobstructed from view from the public right-of-way. Entryways below street level should be avoided.



Prominent entrances that are visible from the street and accessible from the public right-of-way create a more inviting and pleasant pedestrian environment.



Building entryways with landscaping that includes shade trees and easy access to the public right-of-way fosters a sense of security and creates a more attractive and pedestrian-friendly atmosphere.



Angeles National Forest

San Gabriel Mountains

City of San Fernando

Granada Hills - Knollwood

Mission Hills - Panorama City - North Hills

Sunland - Tujunga - Lake View Terrace - Shadow Hills - East La Tuna Canyon

Figure 3-4 Multiple Family Residential Areas Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

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Legend:

- Low Medium I
- Low Medium II
- Medium
- Railroad
- MetroLink Station
- Streets
- Freeways
- Community Plan Boundary

Not to Scale

Goal LU11: Multiple family developments, including small lot subdivisions, apartments, and condominiums, that exhibit the architectural characteristics and qualities that distinguish Sylmar, while incorporating complementary design elements and appropriate transitions.

- LU11.1 **Character and Design.** Create well-designed multiple family dwelling units and buildings that reflect a high level of architectural and landscape quality, both within interior courtyards and in exterior areas, to enhance the public realm.
- LU11.2 **Building Orientation.** Design small lot subdivisions, low-rise townhomes, apartment buildings, and condominiums to ensure that all street facing units have a primary entrance facing the street. For medium density buildings without ground floor entrances for individual units, create a prominent ground or first floor entry, such as a highly visible lobby or atrium.
- LU11.3 **Setbacks.** Apply additional setbacks for multi-family projects adjacent to single-family and equinekeeping lots to ensure that new buildings are compatible with existing structures in the surrounding area.
- LU11.4 **Density Transitions.** Develop small lot subdivisions on multi-family lots adjacent to single-family homes to serve as a transitional density and aesthetic buffer.
- LU11.5 **Building Separations.** When designing small lot subdivisions or projects built over two or more lots, provide sufficient space between buildings, articulation along the street frontage, and visual breaks to diminish the



Transitional building heights, complementary design, and consistent setbacks help provide a smooth transition from a commercial center to a neighborhood with multiple family housing.



This sketch illustrates how a new building may be designed to fit into an existing neighborhood with varying building types.

scale and massing. Locate required open space next to lower intensity uses in order to increase building separations and intensify landscaping between the development and adjacent uses.

LU11.6 **Transitions.** Where multi-family projects are adjacent to single-family zones, provide a sensitive transition by maintaining a height compatible with adjacent buildings. Require appropriate transitions in scale where new multiple-family developments abut single-family neighborhoods.

LU11.7 **Parking and Driveways.** Minimize the appearance of driveways and parking areas, maintain continuity of the sidewalk, and mitigate the impact of parking visible to the street. Parking and driveways should be located toward the rear or side of buildings and away from the public right-of-way. On corner lots, parking should be oriented as far away from the corner as possible.

LU11.8 **Fences and Walls.** Utilize low-lying, open fence design and limit walls and fences within the street-side setback areas in order to provide front entries that are visible from the street. Attractively landscape the setback areas to screen and enhance their appearance.



Architectural features such as arches, articulation and patios, combined with landscaping can help enhance the appearance of multiple family housing, as well as maintain small scale neighborhood settings.



Open entryways, courtyards, attention to architectural integrity, and abundant landscaping will help maintain the small scale character that is prominent in Sylmar.

Design Guidelines for Residential Areas



The Sylmar Residential design guidelines in this Appendix and the Residential Citywide Design Guidelines establish the minimum level of design that shall be observed in all projects within the Sylmar Community Plan area. The intent of the design guidelines is to promote a stable and pleasant environment for the residents and users of the community. The design guidelines are intended to ensure that new structures, alterations and renovations to existing structures make a positive aesthetic contribution to the built environment, provide adequate public amenities and increase neighborhood identity. The overall goal and purpose of the design guidelines are to improve the community's identity and livability and to create unique, inviting and safe public spaces.

Early consultation with Planning staff, including consideration of long-term maintenance, is one of the essential way of assuring implementation of Citywide and these Sylmar Residential Design Guidelines

Character and Design

- G1. Ranch-style architectural details such as the use of low-pitched and hipped roofs, deep-set eaves, large picture windows, recessed entryways, varying front setback planes, wooden or brick exterior wall siding, and/or L-shaped design should be incorporated to encourage and enhance the traditional post-World War II character found in many of Sylmar's single-family residential neighborhoods.
- G2. The suburban character of the neighborhood should be maintained by configuring buildings to front public streets, not driveways. Corner buildings should be designed to be prominent by building to both the front and side property lines facing a street. In detached condominiums or small lot subdivisions, the unit closest to the primary street should be oriented towards the street.

Design Guidelines for Residential Areas (continued)



- G3. New development should be changed so that it continues to engage the street, sidewalk, and public realm by providing individual entrances, large windows, porches, or other entry features to face a street.
- G4. Compatible heights of buildings should be maintained with adjacent and nearby buildings to help preserve the existing low-lying character of Sylmar's single-family residential neighborhoods. Second floor or higher stepbacks should be consistent with prevailing or adjacent buildings.
- G5. Development entryways should be situated within three feet above the grade of the sidewalk to avoid buildings and entryways that tower above or are sunken below the street.
- G6. Existing neighborhood setbacks should be matched and on streets with varying setbacks, align dwellings between the existing maximum and minimum setback.
- G7. Building volumes and façade articulation should be modulated to help convey a sense of individual units and enhance the pedestrian experience. To break massing, multiple planes, stepbacks and architectural treatments such as recessed windows, columns, moldings, and projections should be used. The heights and rooflines should be varied, and offsets in wall planes on all elevations should be used to reduce the visual scale and provide visual interest to a building and individual units.
- G8. The repetitive use of a single building configuration or façade design should be avoided. Provide rhythm to building elevations which adds unity and interest without monotony. Architectural features, such as balconies, porches, decks, awnings, arcades, trellises, color, materials, and diverse roof forms should be used. Landscape features, such as trees, shrubs, and vines should be used to create articulation and a diverse building façade.
- G9. A collection of buildings should be arranged to frame outdoor places, such as a landscaped focal point or courtyard.

Design Guidelines for Residential Areas (continued)



See G10-G11



See G14

Circulation and Parking

- G10. Prevailing garage location and driveway patterns should be considered and incorporated into the design scheme. Innovative design schemes are encouraged that minimize the visual impact of garages facing the street.
- G11. The appearance of parking areas should be minimized by locating parking areas to the rear of buildings and/or providing parking underground, landscaping visible parking areas. Parking areas should not be sited on corners adjacent to intersections.
- G12. Decorative walls and/or landscaping should be used to buffer residential uses from parking areas and structures. Parking structure exteriors should be designed to match the style, materials and color of the main building to integrate parking with the design of buildings.
- G13. The number of curb cuts and width of driveways should be the minimum required by the Department of Transportation (DOT).
- G14. Pedestrian pathways should be separated from auto circulation routes by providing landscaped sidewalks and walkways from sidewalk for homes not adjacent to the street. A change in grade, materials, textures or colors should be used to improve pedestrian visibility and safety. The amount of elevation changes should be minimized through careful grading so as to facilitate disabled access.

Design Guidelines for Residential Areas (continued)



See G15-G17

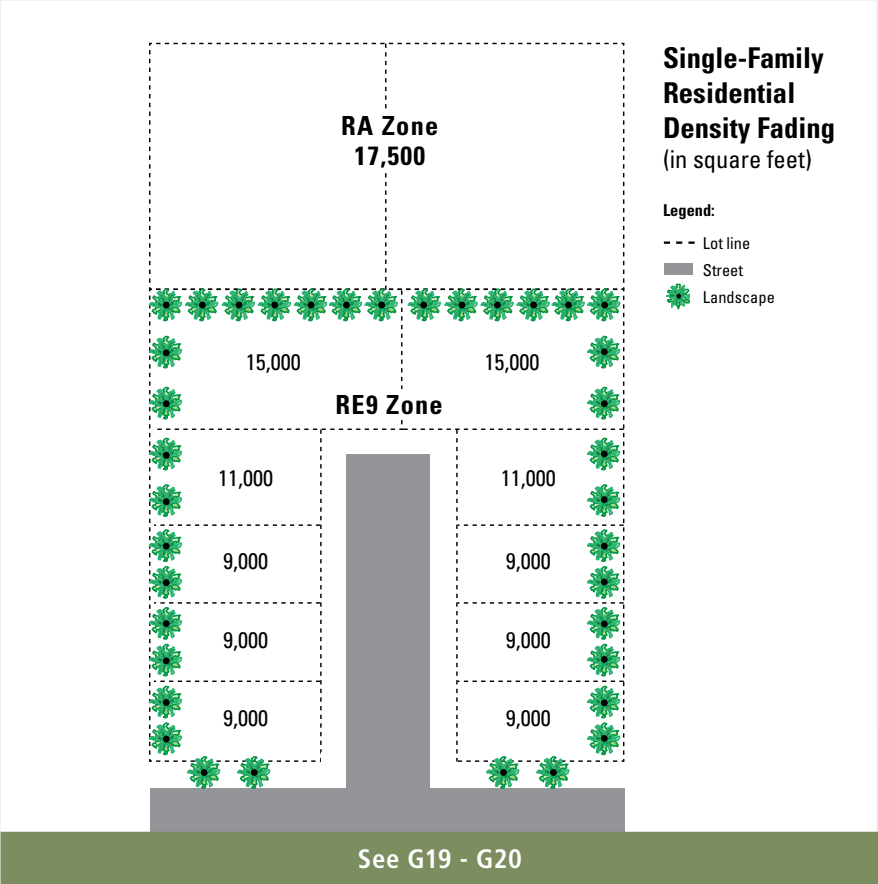


See G15

Landscaping, Fences, and Walls

- G15. Open and inviting yard setbacks and parkways should be provided that are landscaped with a variety of native and/or drought tolerant plant materials, which contribute to the suburban character of this residential community.
- G16. Gated or walled communities should not isolate the project from the neighborhood and surrounding community.
- G17. Low-lying, open fence design should be used. Walls and fences within the street-side setback areas should be limited to 42 inches in height in order to provide front entries that are visible from the street. The setback areas should be attractively landscaped with native and/or drought tolerant plants to screen and enhance their appearance.
- G18. Barbed wire and razor wire fencing should not be used. Alternatives to chain link fencing should be considered.

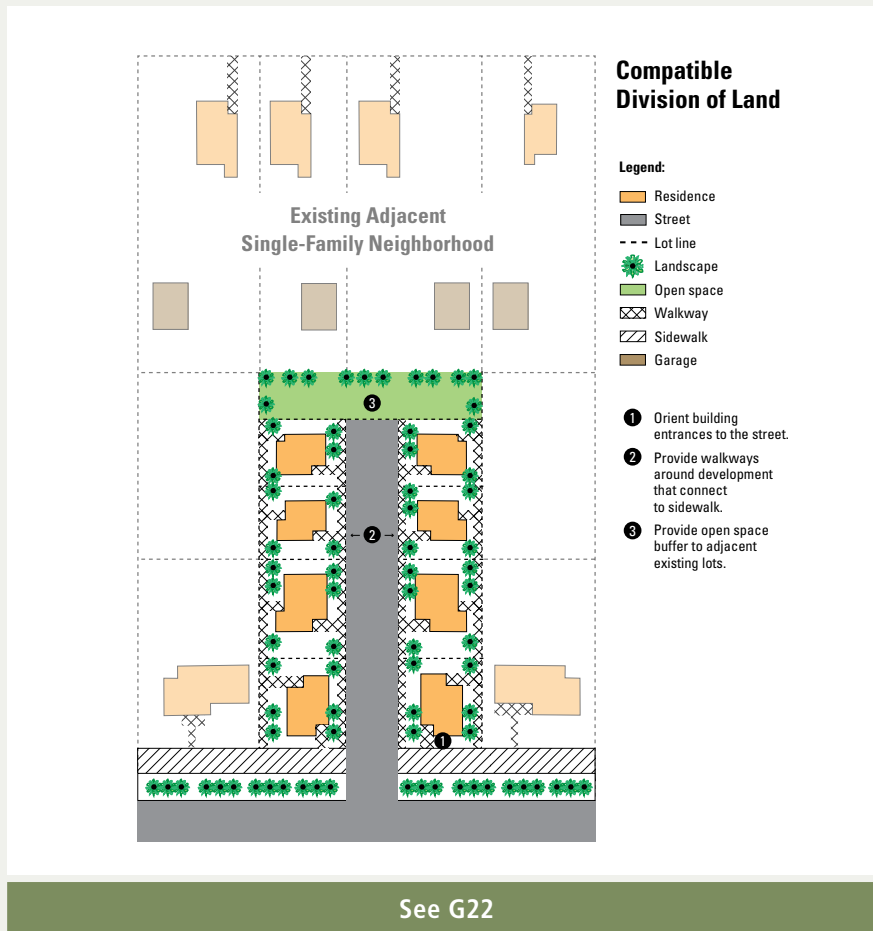
Design Guidelines for Residential Areas (continued)



Neighborhood Transitions

- G19. Lots should be increased in size so as to more closely conform to the size of existing contiguous lots or nearby parcels.
- G20. New development should be transitioned with regard to lot size (density fading) and width so that new parcels are compatible with existing adjacent lots and surrounding neighbors.
- G21. Buildings setbacks for the zoning district should be considered a minimum. Larger setbacks should be provided for multiple-family projects adjacent to single-family and equinekeeping lots.

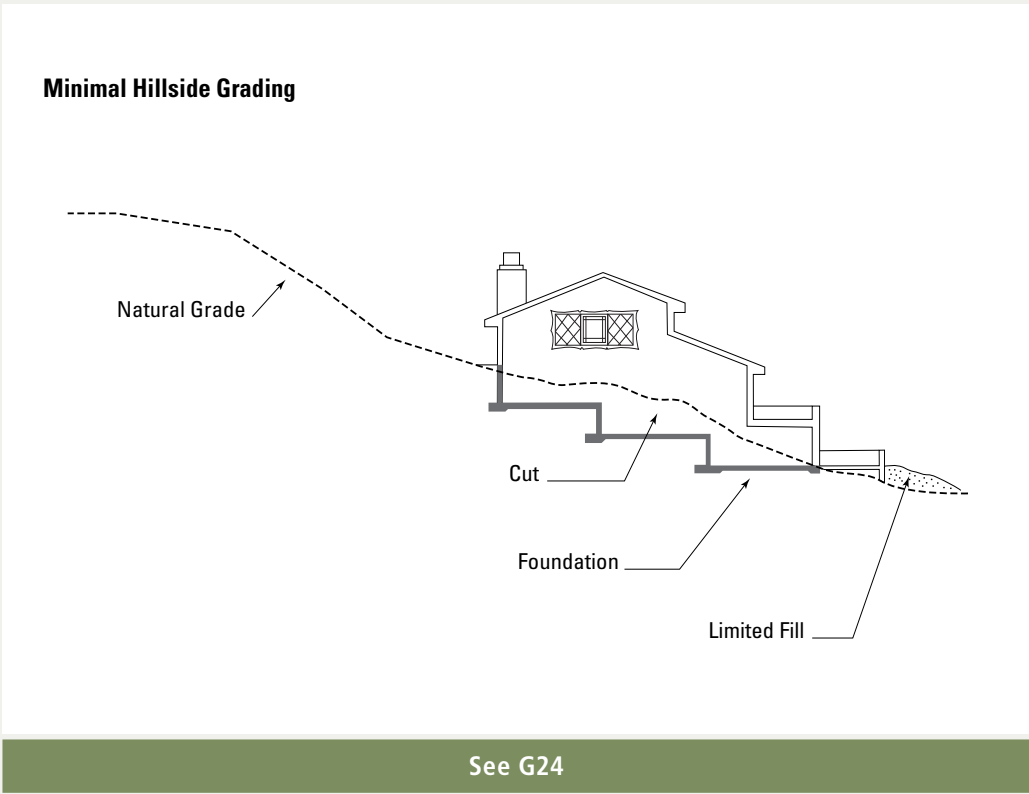
Design Guidelines for Residential Areas (continued)



G22. Open space should be provided and located next to lower intensity uses in order to increase building separations and intensify landscaping between the development and adjacent uses to help mitigate impacts, particularly visual intrusion on private outdoor space of adjacent backyards.

G23. Building heights and mass should be positioned according to the intensity of the adjacent use. Shorter buildings with small footprints should be sited near lower intensity uses while taller, more massive buildings should be located near higher intensity uses and toward the center of the site. Shorter walls should be sited near small structures.

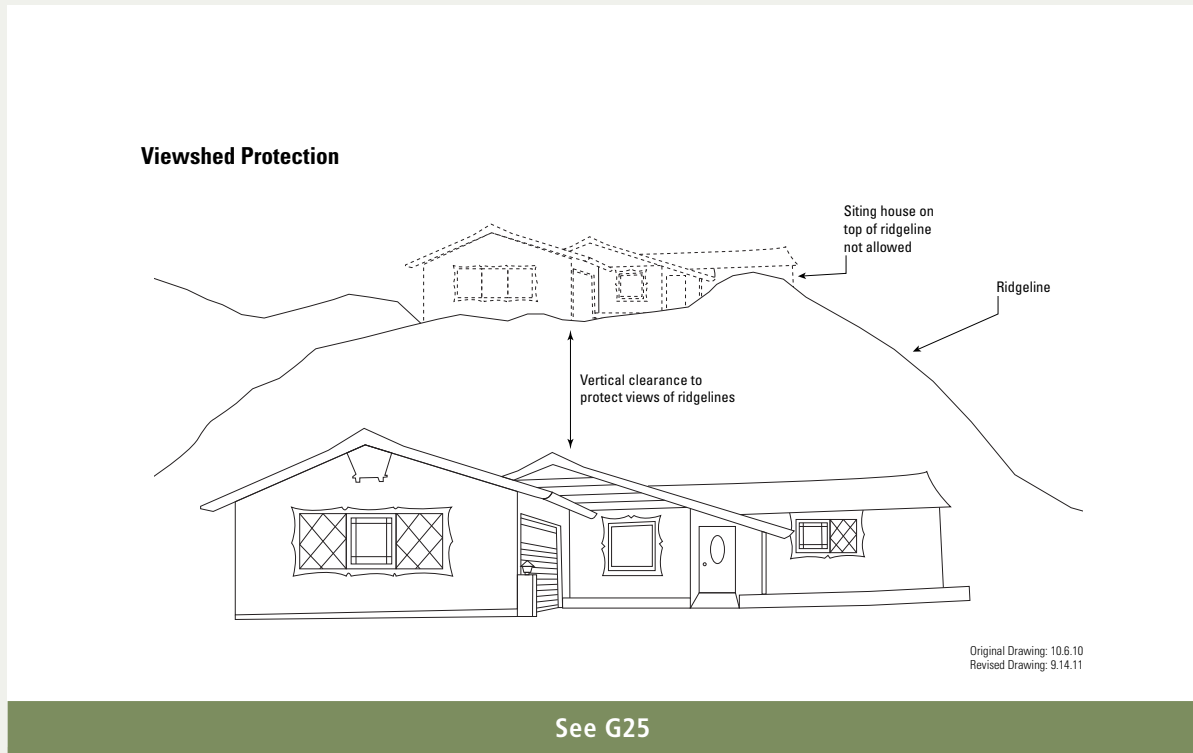
Design Guidelines for Residential Areas (continued)



Hillside Neighborhoods

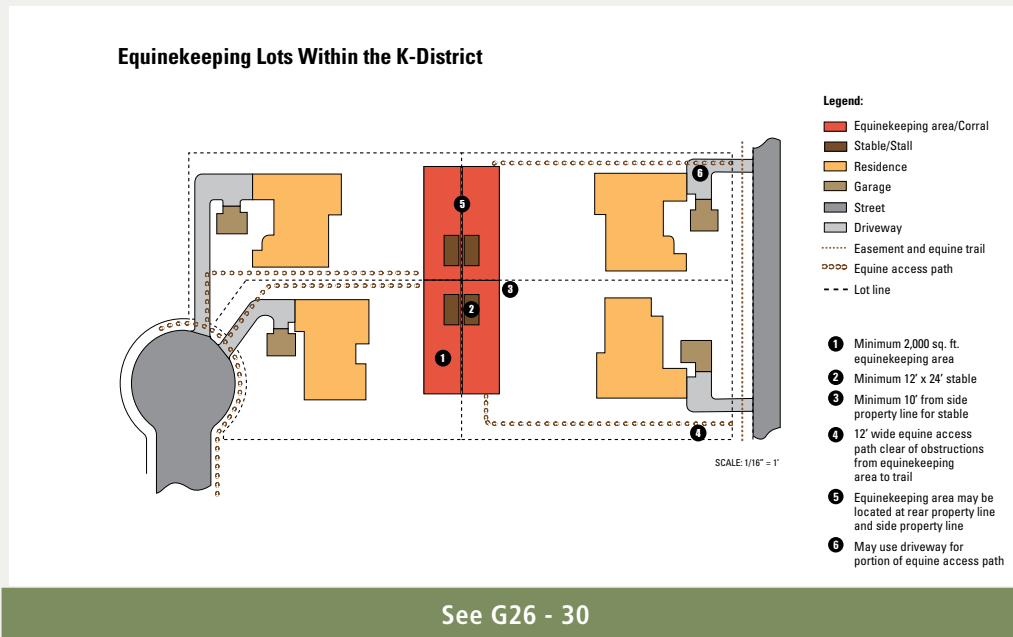
G24. Step buildings up or down the hill to retain the natural grade and to limit the amount of grading required.

Design Guidelines for Residential Areas (continued)



G25. Maintain a vertical clearance between ridgelines and development and site projects below ridgelines in order to maintain and preserve scenic viewsheds with the community. Siting houses on top of ridgelines is not permitted.

Design Guidelines for Residential Areas (continued)



Equestrian-Oriented Neighborhoods

- G26. A 2,000 contiguous square-foot level pad area should be maintained with a minimum width of 24 feet, including a 12'x24' (288-square foot) equine stable area. The pad area should include a 12'x12' (144-square foot) area for storage of feed and equipment, should be graded to permit quick and adequate drainage, and should be permanently set aside for equine use. No permanent structure, including swimming pools and tennis courts, should be constructed or located within any portion of the pad area. Such structures may be permitted outside of the required equine stable and pad areas.
- G27. On small lot subdivision lots, communal equestrian facilities are encouraged in townhouse and detached condominium developments.
- G28. Equine pad and stable areas may be located at the rear property line. Reduced front yard setbacks may be considered to enlarge the backyard area for the purposes of optimizing the intended use of the level pad area and minimizing overall grading and to allow for a better layout for equines and is allowed as long as the reduction does not endanger the equinekeeping use.
- G29. On all lots, the equine pad area may be located within required side yard setbacks depending on the unique features of a site.
- G30. Where access is taken from the front facing street, a 12-foot wide equine access path should be provided. A driveway which is a minimum of 12-feet in width may function dually as an equine access path for the portion of the path that extends from the street to the end of the driveway. Permeable driveway materials are preferred.

Design Guidelines for Residential Areas (continued)



- G31. Where lot access is taken from a side or rear yard of the lot, a gate should be installed to provide direct access for the rider to the trail, sidewalk, or street.
- G32. An adequate on-site parking area should be provided for equine trailers preferably adjacent to the equinekeeping area near the driveway. It is desirable that the parking surface is permeable.
- G33. Utilize decomposed granite, grasscrete, or other similar permeable material to construct the equine access path and parking area for equine trailers.
- G34. On hillside equinekeeping lots, the combined pad area for the primary residence and equinekeeping area shall be a minimum of 11,000 square-feet to ensure adequate separation and to permit construction of larger residential dwellings.

Commercial Areas

Commercial uses represent a small proportion of Sylmar and are located primarily along boulevards with nodes of varying sizes at specific intersections, as shown in Figure 3-5, Commercial Areas. Four Community Centers, four Neighborhood Districts, two Mixed-Use Boulevards, and one Transit-Oriented District serve as focal points for surrounding residents and visitors, providing neighborhood services and retail, including grocery stores, restaurants, small professional offices, and other similar uses and services. Other general commercial uses are dispersed throughout the community and provide important goods and services to nearby residents. Auto-related uses are located throughout the community, along major corridors near freeway on-and-off-ramps.



Commercial development along Foothill Boulevard with parking located at the rear and pedestrian entrances business directly from the sidewalk.



Pedestrian friendly commercial development with ground floor commercial retail and office space above.

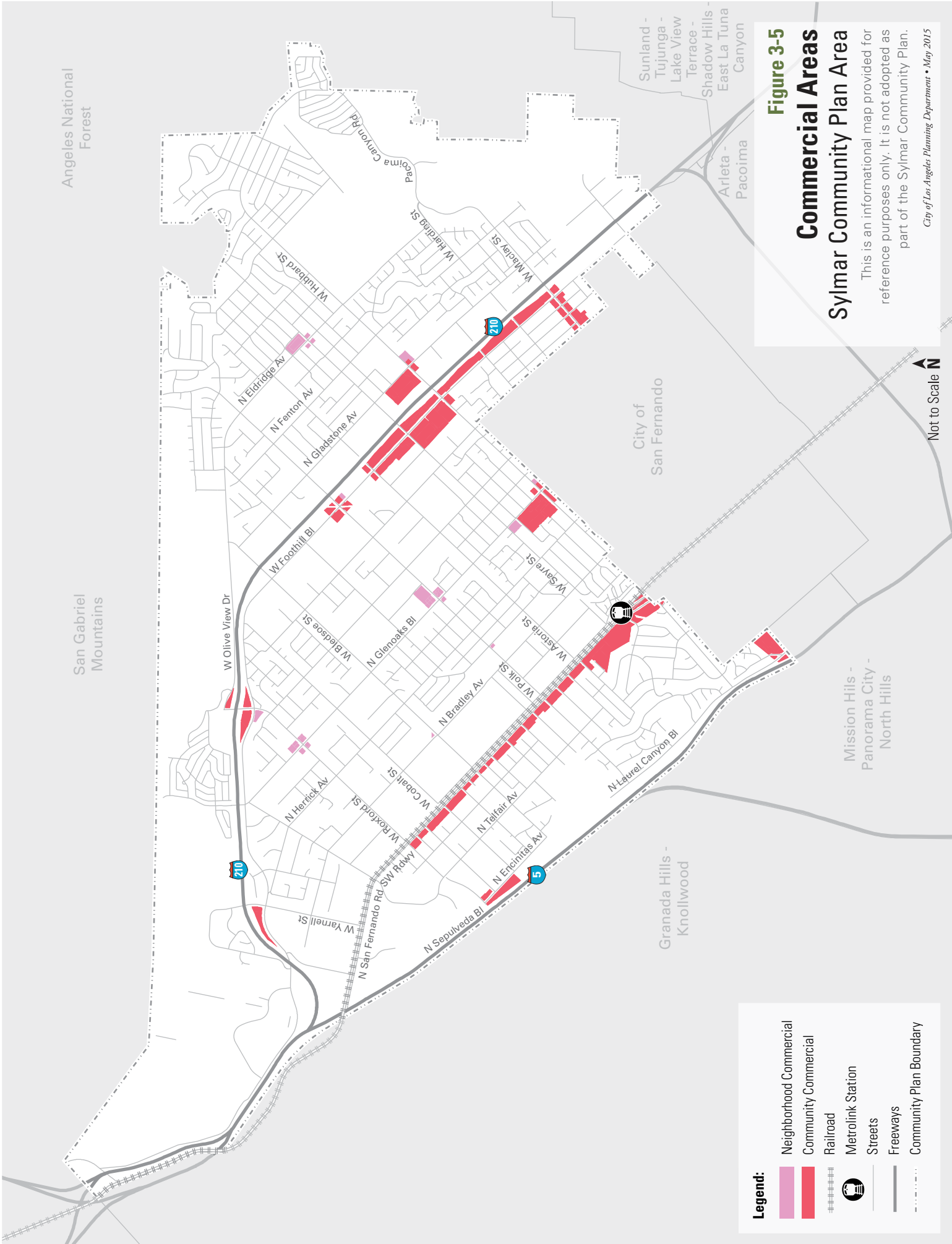


Figure 3-5
Commercial Areas
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

City of Los Angeles Planning Department • May 2015

Legend:

- Neighborhood Commercial
- Community Commercial
- Railroad
- M Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Not to Scale

Commercial Issues and Opportunities

As part of the Sylmar Community Plan update process, community members and stakeholders were given opportunities to comment on the proposed changes. Issues of opportunity areas were identified as a result of public input and goals and policies were developed to specifically address them. Following is a summary of key issues and opportunities in Sylmar:



Public open space that caters to families.



Outdoor dining areas in commercial developments that provide proper lighting, planters and other landscaping elements create a more friendly and walkable environment.



Emphasize the use of Spanish style-architecture and design elements, including earth tone colors, window awnings, and a mixture of landscaping.



Outdoor seating areas associated with commercial and community-serving land uses promotes pedestrian activity and encourages individuals to utilize these areas.

- The community has a strong interest in preserving existing commercial land. While new housing is desirable, by directing housing development near the Sylmar/San Fernando Metrolink Station and other mixed-use corridors, and to designated residential areas, existing commercial centers and districts can be enhanced and preserved to ensure an adequate level of goods and services are provided throughout the Plan area.
- Residents desire the convenience of neighborhood serving commercial uses and “small town” retail establishments within walking distance of neighborhoods. While many of the existing establishments provide a mix of local goods and services, the overall mix does not serve the community’s full range of residents and lifestyles. There is an opportunity to capitalize on existing underutilized establishments and vacant sites and provide opportunities for small-businesses and local jobs, promote healthy neighborhood activity, and reduce vehicle trips.
- More entertainment–related uses, such as movie theaters, sit-down restaurants, and those that cater to all ages and families, are desirable. The absence of these establishments causes many residents with expendable income to travel outside of Sylmar and the City to find such uses.
- Sylmar needs neighborhood and community gathering areas, such as a civic center or community plaza. New commercial development should provide public open spaces so that residents can shop and socialize.
- Residents desire more attractive commercial development. There is a need for improved commercial design and building materials, an appropriate amount and style of signage, and improved landscaping. Policies and design guidelines for new commercial development and remodels will assist in a transition to an improved pedestrian–friendly environment for the commercial areas in Sylmar.

All Commercial Areas

The goals and policies in this section reflect the community's vision to preserve existing commercial land to ensure an adequate level of goods and services are provided throughout Sylmar. It is also the intent of these goals and policies to promote high-quality commercial development. Each of the Commercial Citywide Design Guidelines should be considered, although not all will be appropriate in every case, as each project will require a unique approach. However, to achieve the goal of excellence in new design, projects should incorporate these guidelines to the maximum extent feasible, in order to meet the intent of the Community Plan.

Goal LU12: Strong and competitive commercial areas that serve the community with a wide range of goods and services, support the local businesses and economy, and provide employment opportunities and revenue to the City.

- LU12.1 **Commercial Preservation.** Protect areas designated and zoned for commercial use so that commercial development and reinvestment is encouraged and the community maintains and increases its employment base. Strongly discourage new residential-only development in commercially designated and zoned areas.
- LU12.2 **Commercial Development.** Locate new commercial uses within existing established commercial areas. Accommodate larger projects within Community Centers and near the Sylmar/San Fernando Metrolink Station to minimize impact on residential neighborhoods and help retain the existing community fabric.
- LU12.3 **Lot Consolidations.** Consolidate and deepen shallow commercial areas in a manner that is compatible with prevailing urban form as a means to stimulate existing businesses and create feasible opportunities for new development that is appropriate in terms of scale and character.
- LU12.4 **Equestrian-Related Services.** Encourage uses which serve the equestrian community, such as equine feed stores and veterinary clinics, to locate within Sylmar.
- LU12.5 **Auto-Related Uses and Services.** Allow for the development of new auto-related uses and services, such as gasoline stations, drive-thru establishments, automobile sales and repair, and storage facilities in specific designated Community Commercial land located near the freeways on- and off-ramps. Expansions and modifications of existing auto-related uses are required to be designed to achieve a high-quality architectural and landscape character.

Goal LU13: Commercial and mixed-use development that complement adjacent and surrounding neighborhoods and contribute to Sylmar’s semi-rural character and agricultural heritage.

- LU13.1 **Neighborhood Compatibility.** Ensure that new development adjacent to residential neighborhoods is designed in a manner that is sensitive to existing and adjacent buildings so that it blends in with the neighborhood context.
- LU13.2 **Height Limits.** Design new commercial structures to be compatible in height with surrounding residential neighborhoods.
- LU13.3 **Transitions.** Provide transitional setbacks and upper floor stepbacks for new projects adjacent to residential uses in order to maintain access to light and air and minimize operational impacts.
- LU13.4 **Parking Structures Adjacent to Residential Uses.** Utilize a decorative wall and landscaped setbacks to buffer residential uses from parking areas and structures. Shield and direct on-site lighting onto driveways and walkways and away from adjacent residential uses.



The visual impact of a commercial parking areas can be reduced by providing a buffer like a planter box with landscaping.



Spanish-style architecture is a common feature in Sylmar’s commercial centers.

LU13.5 **Rural Character.** Reinforce the distinctive semi-rural character of the area by incorporating rural features and equestrian-oriented elements, such as ranch-style architecture, hitching posts, and horse parking areas, in remodels and new development, especially commercial areas within or near an Equinekeeping “K” Supplemental Use District and/or adjacent to a trail. Refer to Chapter 4 for additional trail policies and design guidelines.

LU13.6 **Trail Network and Connections.** Design new development and infrastructure improvement projects that abut or connect with an identified trail to develop and/or improve the Trail Network, as shown on Figure 4-5. Provide amenities that support the equestrian lifestyle, including rider height crossing signals at signalized intersections and signs. Refer to Chapter 4 for additional trail policies and design guidelines.



A local restaurant in Sylmar.



A trail abutting a commercial lot connecting to the Saddletree Ranch Trailhead.

Healthy Communities

A growing body of research has shown that there are connections between development patterns, community design and health outcomes. Crafting a more health-friendly Community Plan is critical to the overall health of a community. The Community Plan takes several steps to make community health a priority by developing land use and development strategies that encourage walking, bicycling, horseback riding, and crime prevention through environmental design; and supporting an active, inclusive, and responsive community where healthy habits are encouraged, rather than discouraged, by the environments we build.

Goal LU14: **Attractive and pedestrian-oriented commercial areas that are safe, easily accessible, and enhance the unique commercial and cultural character of the community.**

- LU14.1 **High-Quality Development.** Design projects to achieve a high level of quality and developed in accordance to the Commercial Citywide Design Guidelines and other applicable design guidelines. Projects are required to incorporate to the maximum extent feasible applicable design guidelines.
- LU14.2 **Building Orientation.** Improve neighborhood character and the pedestrian environment of commercial areas by siting buildings so they interact with the sidewalk and the street, contribute to a sense of human scale, and support ease of accessibility to buildings.
- LU14.3 **Pedestrian Access and Connections.** Provide safe and direct pedestrian entrances from the sidewalk and the street and encourage connections to abutting commercial development. Utilize techniques to increase motorist awareness of pedestrians, such as lighting, raised crosswalks, changes in paving, signage or other devices.
- LU14.4 **Building Façade.** Create and reinforce neighborhood identity and a richer pedestrian environment by incorporating features on the building façade that add visual interest to the environment, architectural elements that add scale and character, and building elements that enhance comfort and security of pedestrians.
- LU14.5 **On-site Landscaping.** Provide landscaped areas with shade trees on-site that complement the character of the built environment, add beauty and visual interest, increase pedestrian comfort, and extend the sense of the public-right-of-way onto the site.
- LU14.6 **Streetscape.** Design developments to create inviting districts with landscaped sidewalks lined with shade trees, street furniture, and other pedestrian amenities (or streetscape features), open with visual and physical permeability, and pedestrian-oriented connections, where local residents will be attracted and encouraged to walk to nearby commercial establishments.

LU14.7 **Public Amenities.** Integrate public amenities such as community meeting rooms, civic auditoriums, childcare, plazas, play areas, and public art and open spaces in new development to create destinations for residents to shop and gather and to foster creativity and the arts.

LU14.8 **Parking Lots and Structures.** Treat parking lots and structures with wall textures, colors, and dimensional modules that are coordinated with the architecture of the main building.

LU14.9 **Off-Street Parking and Driveways.** Ensure that clear and convenient access for pedestrians is not minimized by vehicular needs, eliminate auto-pedestrian conflicts, and maintain the character of a pedestrian-friendly street. Locate parking behind or within structures or otherwise fully or partially screened from public view. Where possible, replace surface parking areas with well-designed structured parking and infill parking areas with multi-story mixed-use buildings. Minimize the number of driveways and limit the width of each driveway to the minimum required.

LU14.10 **Lighting and Graffiti.** Use lighting and graffiti abatement to help reduce street crime and violence and promote a sense of safety and the attractive appearance of commercial centers and parking areas. Install on-site lighting along all pedestrian and vehicular access ways.

LU14.11 **Utilities.** Integrate service elements and infrastructure such as mechanical equipment, trash enclosures and utilities with the design of projects. Locate service elements and infrastructure away from crosswalks or sidewalks and screen and/or enclose equipment in order to enhance the pedestrian experience and aesthetic appeal of the building and overall neighborhood. Underground utilities whenever possible.

LU14.12 **Loading and Service Entrances.** Locate loading and service entrances to the side or rear of the building, away from public view of the streets, and incorporate these facilities into the overall design of the building to minimize the visual impact.

LU14.13 **Signage.** Encourage the removal of existing pole, pylon, and roof-top signs and replace them with more appropriate pedestrian-friendly signage such as monument signs, when renovating and developing commercial sites. Restrict new pole and pylon signs, billboards, and digital or electronic signage in all commercial areas, except in specific-designated commercial areas. All signage shall complement the main structure and contribute to the rural character of the area.



Safe, accessible and attractive landscaped sidewalks encourage pedestrian activity along busy streets.



Mechanical equipment can be screened from public view by using innovative techniques.



Commercial development along Foothill Boulevard provides residents with a mix of services.



Large sidewalks with curb bulb-outs encourage businesses to provide landscaping and orient their entrances towards the public street create a more inviting streetscape to pedestrians and customers.

Goal LU15: A traditional strip corridor with a vibrant town center on Foothill Boulevard that offers a wide variety of neighborhood shopping, services, and recreational opportunities near housing.

- LU15.1 **Corridor Development.** Design new infill development to be compatible with the traditional small-scale, pedestrian-oriented, commercial corridor development pattern that exists along both sides of Foothill Boulevard, by locating building frontages at the front property line and placing on-site parking to the side or rear of buildings so that parking does not dominate the streetscape.
- LU15.2 **Historic Resources and Significant Features.** Preserve historic structures and older buildings that add to the character of the existing traditional low-scale commercial corridor development along Foothill Boulevard.
- LU15.3 **Public Services.** Support the retention of existing public services, such as the post office, and encourage new services to locate in this general area.
- LU15.4 **Central Business District.** Encourage new commercial uses, such as retail, restaurants, and professional services in the Sylmar Central Business District located along both sides of Foothill Boulevard from Hubbard Street to Astoria Street.
- LU15.5 **Office Space.** Encourage the development of commercial office space, primarily for professional services such as legal, advertising, marketing, public relations, real estate, financial services, insurance, accounting, computer programming and architecture on the northeast side of Foothill Boulevard between Hubbard Street and Gridley Avenue.
- LU15.6 **Foothill Streetscape.** Enhance the appearance of Foothill Boulevard from Hubbard Street to Astoria Street by providing landscaped parkways that are lined with a variety of palm trees and deciduous trees.
- LU15.7 **Pedestrian Amenities.** Enhance the pedestrian-friendly environment of Foothill Boulevard by increasing street and property lighting, improving way-finding signage, providing bus stop shelters and other pedestrian amenities. Consider providing sidewalk “pop-outs” at Hubbard Street, Sayre Street, and Astoria Street as part of new developments to further enhance the pedestrian environment.
- LU15.8 **Utilities.** Underground utilities or locate utilities at the rear of the site so as to maintain and enhance the streetscape on both sides of the street.

Goal LU16: Sustainable commercial development that uses environmentally-friendly design and technology and water conservation methods which help to minimize consumption of non-renewable natural resources and replenishes the City's underground basin.

LU16.1 Eco-Friendly Design. Design new buildings to respond to the climate of Sylmar through their orientation, massing, and construction. Consider utilizing passive solar design strategies, such as overhangs and shade trees, orienting building volumes, windows, and second-stories to maximize solar access, constructing well-insulated wall systems, and providing usable covered outdoor areas to generate more comfortable and energy-efficient buildings.

LU16.2 Landscaping. Include sustainable landscape strategies such as using deciduous trees to shade buildings in the summer and allow filtered light to penetrate during the winter, planting native and drought-tolerant shrubs, hedges, and vines to reduce water usage, utilizing permeable surfaces on walkways and outdoor spaces and vegetated swales to cleanse and infiltrate water directly onto the ground, and installing drip irrigation systems to conserve water.

LU16.3 Historic Resources and Significant Features. Adapt and reuse historic structures and older buildings as a sustainable way of conserving the character of Sylmar and reinforcing neighborhood character. Respond to unique natural characteristics such as mature trees and topography, on both the project and adjacent sites.



A commercial development providing a variety of shaded outdoor spaces.



Awnings and trees shade buildings and sidewalks and vegetation helps capture runoff.

Neighborhood Districts

Sylmar contains four main Neighborhood Districts, located throughout the community, providing daily convenience services to people living in nearby residential areas, as shown in Figure 3-6, Neighborhood Districts. Typical establishments found in these areas include markets, barber and beauty shops, laundromats and dry cleaners, restaurants, convenience stores, coffee shops, and small professional offices. These districts contain mostly small-scaled, single-story buildings at key intersections. Neighborhood Districts in Sylmar include the following:

- Eldridge Avenue and Sayre Street
- Glenoaks Boulevard and Polk Street
- Glenoaks Boulevard and Roxford Street
- Glenoaks Boulevard and Sayre Street



A local small business providing neighborhood serving uses.



Gathering spaces in new commercial development projects enhance pedestrian activity.

Goal LU17: Quaint, pedestrian-friendly neighborhood commercial districts that serve surrounding neighborhoods and businesses as local gathering places where people can shop and socialize.

- LU17.1 **Neighborhood-Oriented Uses and Services.** Encourage the retention of existing and the development of new neighborhood services and retail that serve the surrounding residents.
- LU17.2 **Housing.** Prohibit new residential uses in neighborhood commercial districts to maintain an adequate level of neighborhood commercial services within walking distance to existing residential areas.
- LU17.3 **Small-Scale Development.** Design projects to be compatible in scale and character with surrounding neighborhoods so that they do not interrupt the community fabric or street grid. Limit buildings to two stories in height in designated neighborhood commercial districts.
- LU17.4 **Streetscape.** Locate building frontages close to the front property line to create a strong street wall and place on-site parking to the side or rear of buildings so that parking does not dominate the streetscape. Design building facades, including storefronts, to be oriented to the street, limit fences facing a street to 42 inches in height, and provide direct pedestrian access into buildings from sidewalks so that they enhance the public realm and actively engage pedestrians.
- LU17.5 **Public Amenities.** Incorporate outdoor spaces, such as plazas and courtyards that accommodate outdoor dining and other activities. Support sidewalk dining to enhance the pedestrian atmosphere.



Commercial development incorporating the use of earth tone colors and natural materials helps reinforce the community's character.



Outdoor dining areas help improve the visual appearance of commercial districts.

Community Centers

Sylmar contains four major Community Centers located along major corridors that provide multiple types of services and businesses catering to the needs of residents, employees, visitors, and businesses within the community, as shown in Figure 3-7, Community Centers. These Centers are characterized by larger lots and buildings that are set back with parking fronting the street developed in the mid to late-20th century. Community Centers in Sylmar include the following:

- Glenoaks Boulevard and Hubbard Street
- Foothill Boulevard (from Hubbard to Sayre Street)
- Gladstone Boulevard and Hubbard Street
- Laurel Canyon and Rinaldi Street



Existing condition of commercial center with a mix of uses.



Existing shopping center with a landscaped parking area.

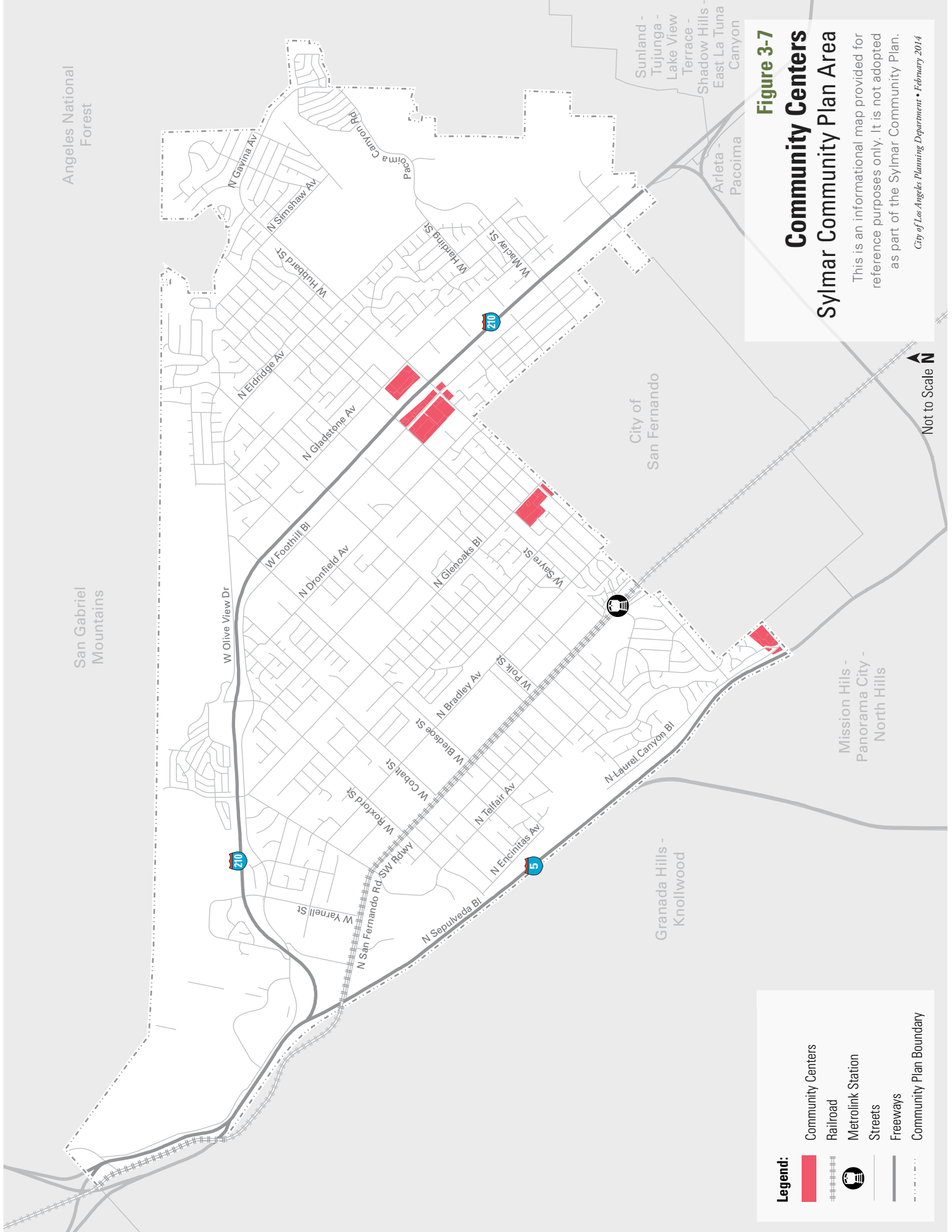


Figure 3-7
Community Centers
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

City of Los Angeles, Planning Department • February 2014

Legend:

- Community Centers
- Railroad
- M Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Not to Scale

Goal LU18: **Vibrant, healthy, and attractive commercial centers that serve as destination points for civic, cultural, and economic life for the adjoining neighborhoods and community, and provide needed goods and services, and jobs in a pedestrian-friendly environment.**

- LU18.1 **Commercial Development.** Promote commercial development by reducing building setbacks adjacent to a public street and reducing off-street parking requirements. Reductions in on-site parking requirements may be considered if it can be determined that the quantity of proposed parking would adequately serve the site or that additional on-street parking or shared parking is provided.
- LU18.2 **Activity-Generating Uses.** Encourage the owners of existing commercial shopping centers to include additional uses, such as restaurants, entertainment, childcare facilities, public meeting rooms, recreation, and public open spaces, which enhance neighborhood activity.
- LU18.3 **Housing.** Permit residential uses above the ground-floor or behind the street frontage. Encourage the development of affordable housing within Community Centers.
- LU18.4 **Surface Parking Lots.** Reduce the negative impacts of vast expanses of asphalt or other paving material by landscaping the parking lot with regularly spaced canopy trees. Require a dedicated landscaped pedestrian pathway or path of travel through the parking lot area.
- LU18.5 **Circulation.** Create and improve pedestrian and bicycle connections by providing dedicated sidewalks connecting businesses within the development, surrounding the development, and connecting the development to nearby neighborhoods. Provide access through barriers separating the development from adjacent neighborhoods.
- LU18.6 **Pole and Pylon Signs.** Limit height of pole and pylon signs to generally 45 feet in all Community Centers.

Transit and Public Benefits

Public transportation plays an integral role in improving a City's prosperity, equity and mobility. It fosters more livable communities by creating corridors that become natural focal points for economic and social activities which help create strong neighborhood centers that are more economically stable, safe and productive.

Transit-friendly, walkable communities reduce reliance on motor vehicles and promote higher levels of physical activity and has the potential to reduce health threats by reducing polluting emissions, providing a safe alternative to automobile travel and contributing to a stress-free commute.

Transit-Oriented District and Mixed-Use Boulevards

Sylmar contains a Transit-Oriented District and two Mixed-Use Boulevards that provide a combination of multiple family housing and commercial uses and services near public transit, as shown in Figure 3-8, Transit-Oriented District and Mixed-Use Boulevards. The highest appropriate development intensities are targeted generally within a quarter mile of the Sylmar/San Fernando Metrolink Station, supported by a mix of housing and neighborhood commercial uses extending along San Fernando Road and Maclay Street. Typical establishments found in these areas include neighborhood commercial uses, public services, cultural facilities, school classrooms, and similar facilities. Sylmar's Transit-Oriented District and Mixed-Use Boulevards include the following:

- San Fernando Road Transit-Oriented District between Hubbard and Astoria Streets
- San Fernando Road Mixed-Use between Astoria and Roxford Streets
- Maclay Street Mixed-Use between Foothill and Glenoaks Boulevards



Landscaped setbacks soften the building heights.



New commercial development within close proximity to transit that provides a mix of community-serving uses is encouraged along San Fernando Road and Maclay Street.



Figure 3-8
Transit-Oriented & Mixed-Use Districts
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

- Legend:**
- Transit Oriented Districts
 - Mixed Use Districts
 - Railroad
 - Metrolink Station
 - Streets
 - Freeways
 - Community Plan Boundary

Not to Scale

Goal LU19: A vibrant Transit-Oriented District and Mixed-Use Boulevards that builds upon the existing residential core and Sylmar’s Metrolink station, and provide a mix of commercial and residential uses, shopping and employment opportunities, open spaces, and civic uses.

- LU19.1 **Transit-Oriented Development.** Encourage projects to include a mix of transit-supportive uses, such as shops, restaurants, offices, housing, and hotels within a quarter mile of the Sylmar/San Fernando Metrolink Station that would serve local residents, employees, businesses, and transit commuters.
- LU19.2 **Ground Floor Retail and Commercial.** Include ground-floor retail/commercial uses fronting San Fernando Road within a quarter mile of the Station. Require ground-floor retail/commercial uses on all corner lots on San Fernando Road between Hubbard and Polk Streets.
- LU19.3 **Mixed-Use Development.** Promote mixed-use projects in proximity to the Sylmar/San Fernando Metrolink Station, along transit corridors, and in identified mixed-use boulevards.
- LU19.4 **Incentives.** Utilize higher Floor Area Ratio (FAR) to incentivize mixed-use development and residential growth near the Sylmar/San Fernando Metrolink Station.
- LU19.5 **Density.** Allow higher density residential development which includes neighborhood service tenants, such as grocery stores and coffee shops, within the project site so that residents have access to everyday uses and minimize automobile trips.



Mixed-used development allows for a building to be utilized for a longer of period of time and gives residents the option to use transit instead of automobiles.



Transit-oriented development with a variety of commercial and residential uses for community members and commuters to enjoy.

- LU19.6 **Housing.** Include a variety of new housing types, such as townhomes, apartments, and condominiums that cater to a diversity of households near public transit as a means of enhancing retail, transit viability, and reducing vehicle trips.
- LU19.7 **Mobile Home Park.** Consider preservation, reconfiguration, and relocation options for the existing mobile home park(s) as part of any planned redevelopment.
- LU19.8 **Community Amenities.** Encourage new development projects, particularly projects which utilize floor area incentives or density bonuses, to incorporate community facilities such as libraries, child care facilities, community meeting rooms, public art and plazas, senior centers, police sub-stations, and/or other appropriate human service facilities.
- LU19.9 **Parking Lots and Structures.** Support opportunities for developing commercial uses on the Sylmar/San Fernando Metrolink Station Park and Ride lot. Ensure that adequate parking space for transit patrons is provided.
- LU19.10 **Shared Parking.** Design large projects with multi-use facilities to share parking and amenities between uses and with other adjacent developments. Consider sharing parking with the Sylmar/San Fernando Metrolink Station Park and Ride lot.
- LU19.11 **On-Street Parking.** Increase on-street parking opportunities, such as angled parking, that support unique shopping experiences and calm traffic movement while providing additional parking for local businesses and services along Maclay Street between Foothill Boulevard and the City Boundary.
- LU19.12 **Local Transit Services.** Promote para-transit and other local shuttle systems, and bicycle amenities that provide access for residents of adjacent neighborhoods.

Reducing Vehicle Miles Traveled

Small incremental increases in station intensity can result in large decreases in per household Vehicle Miles Traveled (VMT) which reduces transportation costs.

Station areas have the potential to reduce VMT and auto-dependence.

Ensuring that a regional transit network connects to major job centers is another key component of reducing auto dependency.

Goal LU20: Visually rich Transit-Oriented Districts and Mixed-Use Boulevards that are vibrant, engage pedestrians, and contribute to street life, vitality, and safety along San Fernando Road and Maclay Street.

- LU20.1 **Building Orientation and Setbacks.** Orient buildings toward the boulevards and maintain a consistent landscaped setback that enlivens the street wall with main entrances fronting the street. Develop the site so that the majority of the building façade is built to the property line in order to create a consistent street wall along the sidewalk.
- LU20.2 **Corner Lots.** Design buildings located at intersections to define and give prominence to the corner on which they are sited, by acknowledging both street facades with façade articulation and detail.
- LU20.3 **Pedestrian-Oriented Architecture.** Incorporate architectural features that encourage walking, such as storefront windows, awnings, and arcades and include architectural features that divide large buildings into smaller, human-scaled components.
- LU20.4 **Architectural Design.** Utilize architecture to create a sense of place and build upon Sylmar’s agricultural history. Projects near the Metrolink Station are encouraged to utilize elements of Spanish-Colonial Revival style architecture.



Window transparency, and entrances oriented toward the street frontage create a pedestrian friendly environment.



Commercial development with outdoor seating areas enhance the streetscape and encourage pedestrian activity.

- LU20.5 **Streetscape.** Support efforts to continue to improve the appearance and safety along San Fernando Road and Maclay Street through distinctive streetscapes and unified landscape treatments that prioritize pedestrians. The revitalized streets should include large deciduous shade trees punctuated by palm trees and unique street lights to help distinguish these areas. Support the development of Streetscape Plans along San Fernando Road and Maclay Street.
- LU20.6 **Connectivity.** Improve the connectivity along San Fernando Road by implementing techniques to make the pedestrian environment more pleasant on longer blocks or find ways to break down the scale of these superblocks with new pedestrian connections, such as incorporating streetscape and hardscaped improvements and private easements for public use.
- LU20.7 **Metrolink Connections.** Improvements for this area should foster a connection between the Metrolink Station, railroad tracks, and the major arterial intersection of San Fernando Road. Pedestrian connections across San Fernando Road and a multi-modal transit plaza to promote transit access to and from the existing rail station is highly supported and encouraged.



Streetscape improvements have helped strengthen the appearance of the Sylmar/San Fernando Metrolink Station and the surrounding vicinity.



Landscaped medians along San Fernando Road near the Metrolink Station.

Industrial Areas

Sylmar has a significant amount of industrial uses, predominantly located in the southeast corner and northwest portion of the community, near the freeways, as shown on Figure 3-10, Industrial Areas. The industrial areas consist of mostly warehouse and distribution centers, light manufacturing, and research and development, providing local residents and surrounding communities with employment opportunities. Sylmar's industrial base is well-established and industrial land will be preserved to help maintain a core employment base.

Industrial Issues and Opportunities

Throughout the Sylmar Community Plan update process, opportunities were provided for community members and stakeholders to comment on the proposed plan changes. As a result of public input and staff research, issues and opportunities were identified. Following is a summary of key issues and opportunities identified:

- One of Sylmar's strongest assets is its thriving industrial sector. Preserving Sylmar's industrial land for existing and new businesses helps to contribute to the community and the city's well-being, and provides the City with the necessary revenue to deliver the services desired by its residents.
- There are opportunities for "green" technology and "green" companies, research and development businesses, food production, artisan industries and media production to locate in Sylmar.



High quality design of industrial buildings helps enhance the character of the industrial and business parks in Sylmar.



Sunset Farms Business Park is one of the largest business centers providing jobs in the north San Fernando Valley.

- The Bradley Avenue industrial area is fully occupied with industrial uses but is viewed as being underutilized. It has the potential for being developed of introducing business park-related amenities like restaurants, café, open space, such as a plaza, and other business-supporting amenities, and job training centers or targeted incubator facilities.
- The industrial district on Arroyo Street, north of Foothill Boulevard, is a stable district comprised of older industrial businesses. However, the buildings front a very narrow road making it very difficult to navigate large trucks and vehicles conducting business in this area. Limiting delivery on-site or to non-peak hours and requiring sufficient on-site parking for employees, customers, and visitors and restricting off-site parking during peak hours could potentially alleviate some of this problem.
- Landscaped streetscapes are an important characteristic of Sylmar. Many of the industrial parks and districts are lined with canopy trees and parkways which contribute to the semi-rural character of the area. New development should enhance and maintain this attribute.
- Some of the older industrial districts are unsightly for businesses. There are tremendous opportunities to enhance these areas by providing landscaped entrances, removing or replacing chain link fences fronting public streets, and requiring parking at rear or side. Parking directly between buildings and the street should be avoided.



Landscaped walkways with a mix shrubs and shade trees create inviting entrances for the public.



Newer industrial parks in Sylmar provide research and development as well as other “green” industrial land uses to the community plan area.



Figure 3-9
Industrial Areas
Sylmar Community Plan Area
 This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Not to Scale

Legend:

- Limited Industrial
- Light Industrial
- Railroad
- ⊞ Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Goal LU21: Industrial uses that provide job opportunities, particularly for residents, and minimize environmental and visual impacts to the community.

- LU21.1 **Employment Preservation.** Retain industrial planned designations to maintain the industrial employment base for existing and new businesses that provide higher-skilled and high wage manufacturing and research/development jobs.
- LU21.2 **Industrial Land Retention.** Restrict large industrially planned parcels located in predominantly industrial areas to be developed with uses that support the industrial base of the City and community.
- LU21.3 **Residential Restrictions.** Prohibit all residential uses in the MR1 zones, except for accessory residential uses.
- LU21.4 **Job Safety.** Ensure that industrial land uses are safe for human health and the environment and that they provide a robust source of employment.
- LU21.5 **On-site Parking.** Require adequate customer and employee parking be provided for all types of industrial and manufacturing facilities, and that truck traffic and parking be restricted from residential areas.

Job-Housing Balance

Trends show that people are traveling longer distances to their places of employment. By planning communities with better balances of jobs and housing units, work trips can be shortened and therefore have a significant impact on vehicle miles traveled (VMT). A job-housing balance can also help reduce tailpipe emissions and improve air quality, reduce traffic congestion and lower personal transportation costs, slower increases in housing costs, and the potential to reduce public costs for new road construction and improvements. Additional benefits from balancing jobs and housing include achieving greater social equity, reducing class segregation, and creating interesting, pedestrian-oriented places.



Sunset Farms Business Park provides a jobs housing balance with a mix of housing options in the form of apartments and condos.



Curvilinear streets within industrial areas helps with the transition of uses and provides visual enhancement.



Landscaped setbacks and mature street trees that provide shade aids in the beautification of industrial land uses within the community.

Goal LU22: Land use compatibility between industrial, residential and commercial uses, improving the aesthetic quality and design of industrial areas.

- LU22.1 **High-Quality Development.** Design projects to achieve a high level of quality, and developed in accordance to the Industrial Citywide Design Guidelines and other applicable design guidelines. Projects are required to incorporate to the maximum extent feasible applicable design guidelines.
- LU22.2 **Neighborhood Compatibility.** Require design techniques, such as appropriate building orientation and scale, landscaping, buffering, noise insulation and increased setbacks, in the development of new industrial properties adjacent to non-industrial uses to improve land use compatibility and to enhance the physical environment.
- LU22.3 **Transitional Uses.** Require transitions for industrial uses, including scale, massing, and setbacks, in those areas in close proximity to residential neighborhoods.
- LU22.4 **Landscaped Buffers.** Incorporate landscaped buffers between the buildings and abutting residential properties. Methods to buffer projects should include a combination of increased setbacks, landscaping, berms and/or screening, and fencing.
- LU22.5 **Street Beautification.** Encourage streetscape improvements such as street trees, sidewalks, landscaping, lighting, and undergrounding of utilities. Projects within the two industrial parks, Telfair Avenue and Balboa Boulevard, should maintain the existing landscaped pattern.
- LU22.6 **Walls and Fences.** Design walls, fences, and screens used for concealment purposes to blend with the site’s architectural style and soften them with clinging vines or shrubs. When security fencing is required, utilize a combination of solid pillars, open iron fence elements, solid wall segments, and grillwork. Chain link fencing and barbed wire fencing is not allowed.
- LU22.7 **Integration of Utilities.** Integrate service elements and infrastructure such as mechanical equipment, trash enclosures and utilities with the design of projects. Locate service elements and infrastructure away from crosswalks or sidewalks and screen and/or enclose equipment in order to enhance the pedestrian experience and aesthetic appeal of the building and overall neighborhood. Underground utilities whenever possible.

Goal LU23: **Environmentally-friendly businesses that offer green jobs, safer neighborhoods, and a healthier environment, and utilize design, technology, and water conservation methods which help minimize consumption of non-renewable natural resources and replenishes the City’s underground basin.**

LU23.1 **Environmentally Friendly Businesses.** Support “green” business growth and encourage the replacement of polluting land use activities with environmentally-friendly businesses.

LU23.2 **Sustainable Industry.** Incentivize development opportunities for businesses that employ “green” or clean technologies, building practices, and processes.

LU23.3 **Eco-Friendly Design.** Design new buildings to respond to the climate of Sylmar through their orientation, massing, and construction. Consider utilizing passive solar design strategies, such as overhangs and shade trees, orienting building volumes, windows, and second-stories to maximize solar access, constructing well-insulated wall systems, and providing usable covered outdoor areas to generate more comfortable and energy-efficient buildings.

LU23.4 **Landscaping.** Include sustainable landscape strategies such as using deciduous trees to shade buildings in the summer and allow filtered light to penetrate during the winter, planting native and drought-tolerant shrubs, hedges, and vines to reduce water usage, utilizing permeable surfaces on walkways and outdoor spaces and vegetated swales to cleanse and infiltrate water directly onto the ground, and installing drip irrigation systems to conserve water.



Encourage and support environmentally friendly businesses within the industrial areas of the community.



An industrial building with wall-climbing vines.

Historic, Architectural, and Cultural Resources



Heritage Park offers passive open space and creatively tells the story of the history of the area.



Heritage Park also provides a Mission-style plaza, trail that winds through the park, and an abundance of drought-tolerant plants.

The community of Sylmar has a rich built history based on its agricultural roots and equestrian heritage – from the first City aqueduct to the second oldest cemetery in the San Fernando Valley. The preservation of historic resources protects this built legacy, ensuring continuity and the retention of the community’s collective memory. Historic preservation also offers economic benefits, as communities throughout the nation have used preservation as a successful tool to promote revitalization and economic development.

Citywide historic preservation policies have been established through a Cultural Heritage Master Plan, the Conservation Element of the General Plan, and the work plan of the Department of City Planning’s Office of Historic Resources (OHR). In addition to these policies, the City of Los Angeles has launched SurveyLA — an ambitious citywide survey to document historic resources across Los Angeles. Results of SurveyLA will inform the development of policies to preserve historically and architecturally significant buildings that have not yet achieved official historic designation. SurveyLA is coordinated by Office of Historic Resources.

There are a number of potentially significant historical resources located within the community of Sylmar. These include building types which represent several phases of the architectural evolution in the City of Los Angeles, such as the California Ranch House, Farmhouse, Spanish Colonial Revival, and American Colonial Revival.

The Community Plan contains goals and policies that aim to preserve historic resources that protect the built legacy, ensuring continuity and the retention of the community’s collective memory. The Community Plan reflects the Community’s vision to preserve the areas agricultural and equestrian heritage and its built legacy and ensures that the area’s significant historic resources are protected, preserved, and or enhanced.

As of 2012, three sites have been designated as Historic-Cultural Monuments (HCM) and/or California State Historical Landmarks (CHL), in recognition of their significance in the social and cultural history of the community, See Figure 3-10. The three identified historical sites include:

- San Fernando Mission Wells and Settling Basin
- San Fernando Pioneer Memorial Cemetery
- Los Angeles Aqueduct Cascades

The San Fernando Mission Wells and Settling Basin

The San Fernando Mission Wells and Settling Basin was built in the early nineteenth century in order to have access to water. Located near the Sylmar Metrolink Station, this six-acre well site is the oldest existing source of water supply in the City, other than the Los Angeles River. The Wells are now owned and operated by the Department of Water of Power (DWP) and are still a valuable source of water supply for the City. The Wells were declared Los Angeles Historic-Cultural Monument #50 in 1967.

San Fernando Pioneer Memorial Cemetery

The Pioneer Memorial Cemetery is located at the corner of Foothill Boulevard and Bledsoe Street. This cemetery covers about four-acres of what was once part of Senator Charles Maclay’s ranch. It is the second oldest cemetery in the San Fernando Valley and holds the remains of early pioneers, Civil War Veterans, and Mission Indians. The cemetery was declared a California State Historical Landmark in 1961 and Los Angeles Historic-Cultural Monument #586 in 1993.

Los Angeles Aqueduct Cascades

The Los Angeles Aqueduct was completed in 1913 in order to provide the City with a reliable source of water supply. The aqueduct is located just north of the Foothill and Balboa Boulevard intersection and is the terminus of the Los Angeles — Owens River Aqueduct which brings water 338 miles from the eastern slopes of the Sierra Nevada to the City. The aqueduct was declared a California State Historical Landmark in 1958 and Los Angeles Historic-Cultural Monument #742 in 2003.



San Fernando Pioneer Cemetery, Los Angeles Historic-Cultural Monument #586.



Sylmar contains a number of homes that reflect the historic character and architecture of the Sylmar community.



Legend:


- ##### Railroad
-  Metrolink Station
- Streets
- Freeways
- - - - - Community Plan Boundary

Figure 3-10
Historical Sites
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Goal LU24: A community with distinct and historically significant character which values and preserves its historic resources and cultural amenities for future generations to enjoy.

LU24.1 Historic Preservation. Protect, preserve, and enhance identified cultural and historical resources.

LU24.2 Cultural Resources. Support existing cultural and historic resources, such as the Pacoima Wash, Veterans Memorial Park, Stetson Ranch Equestrian Park, and Wilson Canyon Trailhead, as cultural resources for the community.

LU24.3 Resource Identification. Support the completion of SurveyLA or other future comprehensive survey within the Sylmar Community Plan Area. Ensure careful review under the California Environmental Quality Act (CEQA) of project proposals affecting resources identified in the survey as eligible for historic designation.

LU24.4 Rehabilitation. Encourage preservation and rehabilitation of historic resources. Promote the use of the City’s Mills Act Historical Property Contract Program, the Federal Historic Rehabilitation Tax Credit, and the California Historical Building Code. Any project which involves designated historic resources, including the City of Los Angeles Historic-Cultural Monuments shall conform with the Secretary of the Interior’s Standards for Rehabilitation.



Mural art in commercial areas promote a sense of community identity and pride.



Sylmar’s semi-rural character is reflected by residents in unique ways throughout the community.

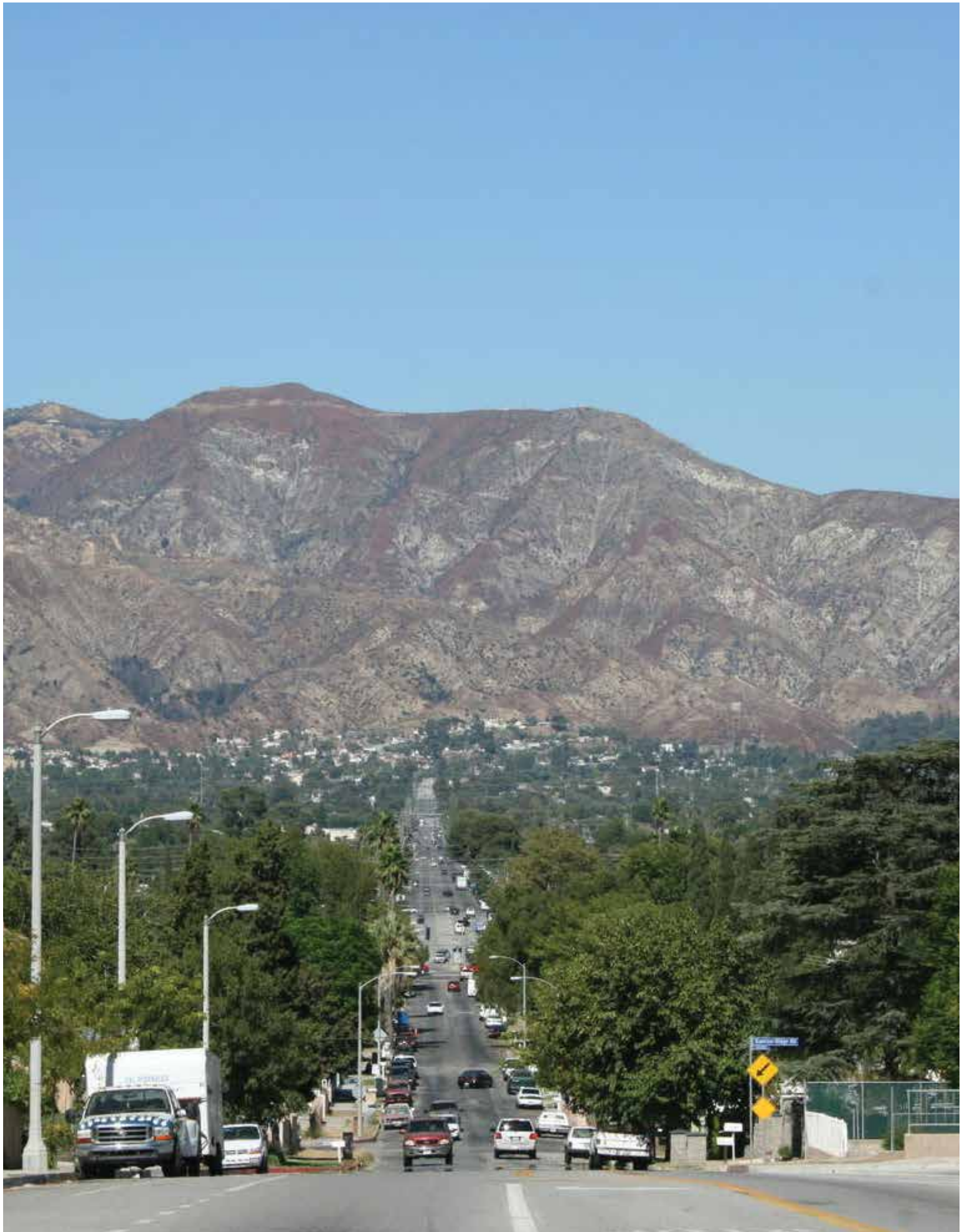
- LU24.5 **Restoration and Re-Use.** Promote the restoration and reuse of existing buildings as a key component of the City’s sustainability policies.
- LU24.6 **Historic Integration.** Encourage the design of new buildings that respect and complement the character of adjacent historic resources.
- LU24.7 **Special Districts.** Support the study of Residential Floor Area (RFA) Special Districts or Community Design Overlays (CDOs) for neighborhoods that retain a cohesive character but are not eligible to become Historic Preservation Overlay Zones (HPOZs).
- LU24.8 **Community Partnerships.** Forge partnerships with relevant neighborhood organizations to advance preservation efforts in the community through educational and informational programs.
- LU24.9 **Partnerships.** Partner with the Los Angeles Housing Department and other agencies to identify new financial resources for rehabilitation grants and loans to low- and moderate-income owners of historic resources.



The Sylmar Business Improvement District sponsored the design and implementation of banners along San Fernando Road that reflect Sylmar’s identity.



It is the intent of this community plan to preserve the stable single family residential areas that reflect the historic character of Sylmar’s development.



Eastward view of Polk Street near Laurel Canyon Boulevard looking toward the San Gabriel Mountains.

Mobility and Public Health

Physical inactivity is increasingly recognized as a public health problem due to the associated increases in obesity, diabetes, cancer, stroke, and heart disease in our communities. A 2007 study by Los Angeles County Department of Public Health found that the communities of Sylmar, Mission Hills Panorama City-North Hills, and Arleta-Pacoima have a 29% prevalence of childhood obesity. The ability to efficiently, safely, and enjoyably walk, ride, or bicycle in one's community can have a significant impact on individual activity levels. This Plan promotes active living through pedestrian, equestrian, and bicycling improvements, increased access to parks and green spaces, and supporting the development of trails and safe routes to school.

*Source: "Preventing childhood obesity: the need to create healthy places. A Cities and Communities Health Report," County of Los Angeles Department of Public Health, October 2007.

Mobility

Whether walking, riding a bike or a horse, taking public transit or driving a car, community members need to find efficient, safe and enjoyable modes of transportation to reach their destinations. "Mobility" is the ability to quickly, comfortably travel within the community and region using one or several modes of transportation. One's mobility is enhanced if a range of practical and affordable travel options are available.

The Sylmar Community Plan recognizes that land use and mobility goals and policies are interdependent. Mobility objectives cannot be achieved without the support of appropriate and complementary development; at the same time, land use and design objectives can be undermined by conflicting mobility policies. Therefore, the mobility goals and policies in this chapter enhance and reinforce the land use and design policies discussed in Chapter Three, while integrating citywide mobility goals, including those established in the Framework, Transportation and other City Elements. These citywide goals include:

- Support a first-class, multi-modal transportation system in which jobs, services and amenities are easily accessible to all residents and visitors, which respects the City's unique communities and neighborhoods and reduces the City's dependence on automobiles.
- Improve air quality, public health, and quality of life through continued investment in rail, transit, bicycle, pedestrian, and trail infrastructure.
- Create a street network that balances the needs of all roadway users, including pedestrians, equestrians, bicyclists, transit riders, and motorists; and which values streets as public open spaces.

This chapter first introduces the concept of “complete streets”, the basis for Sylmar’s multi-modal approach to mobility. Official street standards, modifications to these standards, and street prioritization by mode are also discussed in this section. The remainder of the chapter contains all of Sylmar’s mobility goals, policies, and design guidelines, organized into the following nine sections:

- General Mobility
- Walking
- Bicycling
- Trails
- Scenic Highways
- Public Transit
- Motorized Vehicles
- Goods Movement
- Parking Management

Complete Streets

“Complete streets” are road-ways designed and operated to enable safe, attractive, and comfortable access and travel for all users. Pedestrians, bicyclists, motorists and public transport users of all ages and abilities are able to safely and comfortably move along and across a complete street. In 2007, the State of California adopted the “Complete Streets Act,” which requires local municipalities to plan for the routine accommodation of all roadway users when updating General Plans.



Sylmar is a semi-rural, equestrian community where residents ride horses on a daily basis to get to recreational and open space areas and local retail shops.



The bicycle path on San Fernando Road provides residents with a dedicated environment for cycling and walking.

Streets

Our City's streets serve many different roles within a community. They are a means to get people to places they need to go — via bus, light rail, car, motorcycle, scooter, bicycle, on foot, and even by horseback in some the City's communities. Streets are also places to gather, recreate, shop, exercise, and socialize. They are the backbone of a healthy community and an indicator of a local neighborhood's culture and values. Streets must also provide mobility for our businesses, which often rely on the timely delivery of merchandise to their stores or the ability to deliver services to customer's homes or offices. Furthermore, streets accommodate utility and sewer lines as well as provide a means to collect and transport water on rainy days. Simply stated, daily life demands a great deal from our streets; thus, the sustainable future of our neighborhoods depends on a network of roadways that balance the needs of these multiple interests and functions. Currently, most of the City's streets are devoted primarily to moving vehicular traffic; however, over-dependence on motor vehicles puts communities in a vulnerable economic position and diminishes quality of life. Therefore, this Plan encourages a more balanced, multi-modal approach to mobility in which the community's streets are more equitably shared by all users, termed "complete streets" by the California Complete Streets Act of 2007.



San Fernando Road, a major street in the community, connecting to the Metrolink Station



Residents are commonly seen horseback riding throughout the community.

Standard Street Classifications

The City's streets are organized by standard street classifications, established in the General Plan, as well as standard street dimensions, adopted by the City Planning Commission and Board of Public Works and depicted in the Department of Public Works Standard Plan. Street classifications describe a street's function within the larger street network, while street dimensions assign appropriate street right-of-way widths (comprised of space for sidewalks, parkways, street parking, travel lanes, and medians) for each classification. The City's principal street classifications include:

- **Boulevards I and II.** Boulevards are designed to carry high volumes of vehicular traffic at relatively high speeds. A Boulevard I typically has three lanes of traffic in each direction; while a Boulevard II typically has two lanes. Local automobile access to individual parcels along the street should be limited.
- **Avenues I, II, & III.** Avenues are intended to supplement the through-traffic carrying characteristics of major highways, is designed for fewer daily trips than a Boulevard and typically provides more access to individual parcels. It typically includes two travel lanes in each direction, left turn lanes at signalized intersections, and narrower sidewalks than the Boulevards.
- **Collector Streets** (standard, industrial, and hillside). Collector Streets are moderate-volume, medium-speed roadways that provide access between local streets and higher street classifications.
- **Local Streets** (standard, industrial, and hillside). Local streets are designed to allow local traffic access to their property or destination.

Although street widths for each of the above street classifications are standardized, actual street widths vary because, historically, streets were built to different, narrower standards. In these circumstances, older streets are incrementally widened through street dedications from new development; however, this method of street widening may be impractical or counter to goals of increased pedestrian, bicycle, or development activity. Existing, non-standard street dimensions, types of adjacent land uses, lot depths, and volumes of vehicular, pedestrian, and bicycle activity may all indicate the need for a different street dimension than the adopted standard. In these cases, streets and street segments can be modified to reflect the specific needs of a community. Figure 4-1, Circulation System, delineates Sylmar's street network and establishes right-of-way widths and dedication requirements. In the next section, Table 4-2, Street Modifications, lists the streets that will be modified concurrent with this plan's adoption.



San Fernando Road, a Boulevard III.



Glenoaks Boulevard, an Avenue.



Cobalt Street, a Collector Street.



Gladstone Avenue, a Local Street.

Angeles National Forest

San Gabriel Mountains

City of San Fernando

Granada Hills - Knollwood

Mission Hills - Panorama City - North Hills

Sunland - Tujunga - Lake View Terrace - Shadow Hills - East La Tuna Canyon


















Figure 4-1

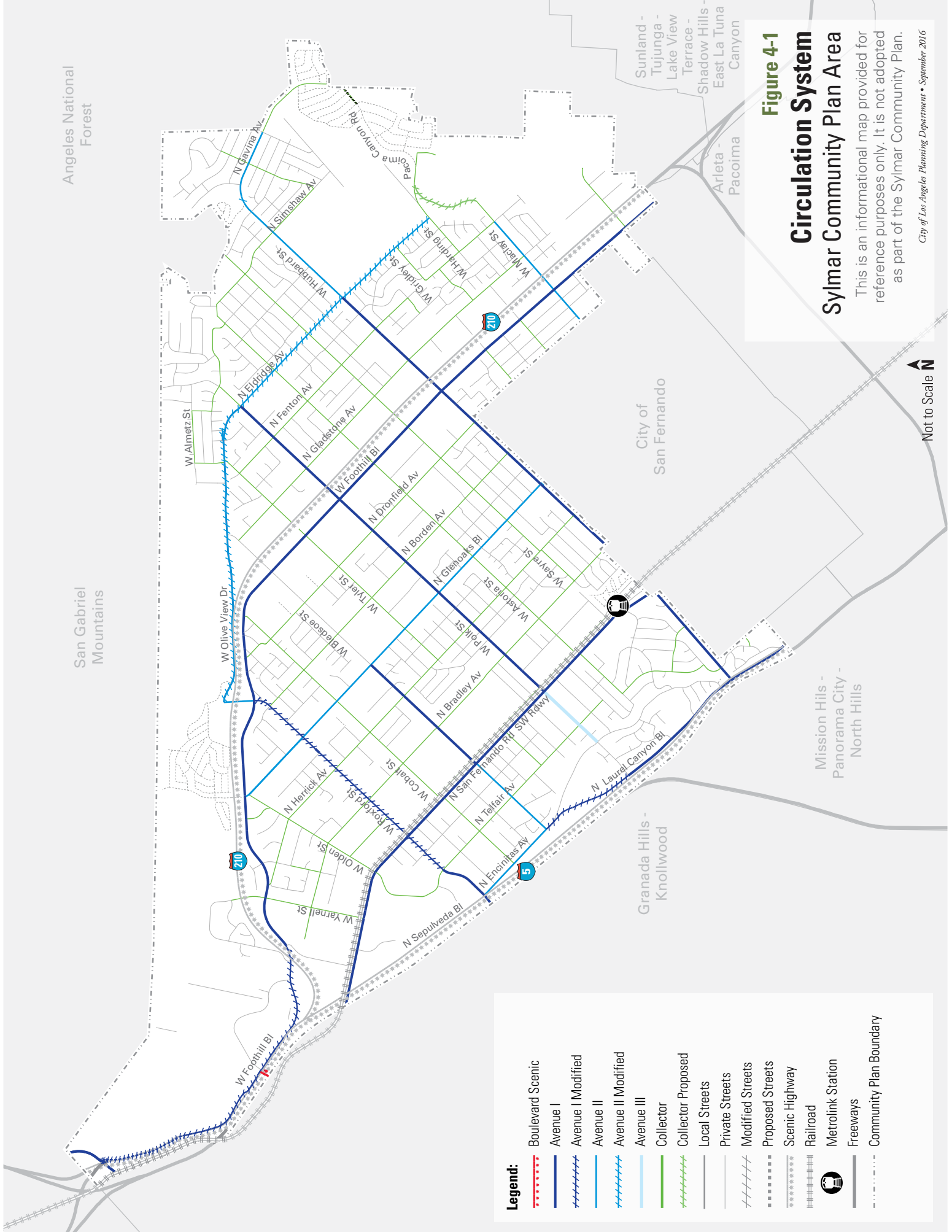
**Circulation System
Sylmar Community Plan Area**

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Not to Scale

Legend:

-  Boulevard Scenic
-  Avenue I
-  Avenue I Modified
-  Avenue II
-  Avenue II Modified
-  Avenue III
-  Collector
-  Collector Proposed
-  Local Streets
-  Private Streets
-  Modified Streets
-  Proposed Streets
-  Scenic Highway
-  Railroad
-  Metrolink Station
-  Freeways
-  Community Plan Boundary



Priority Streets

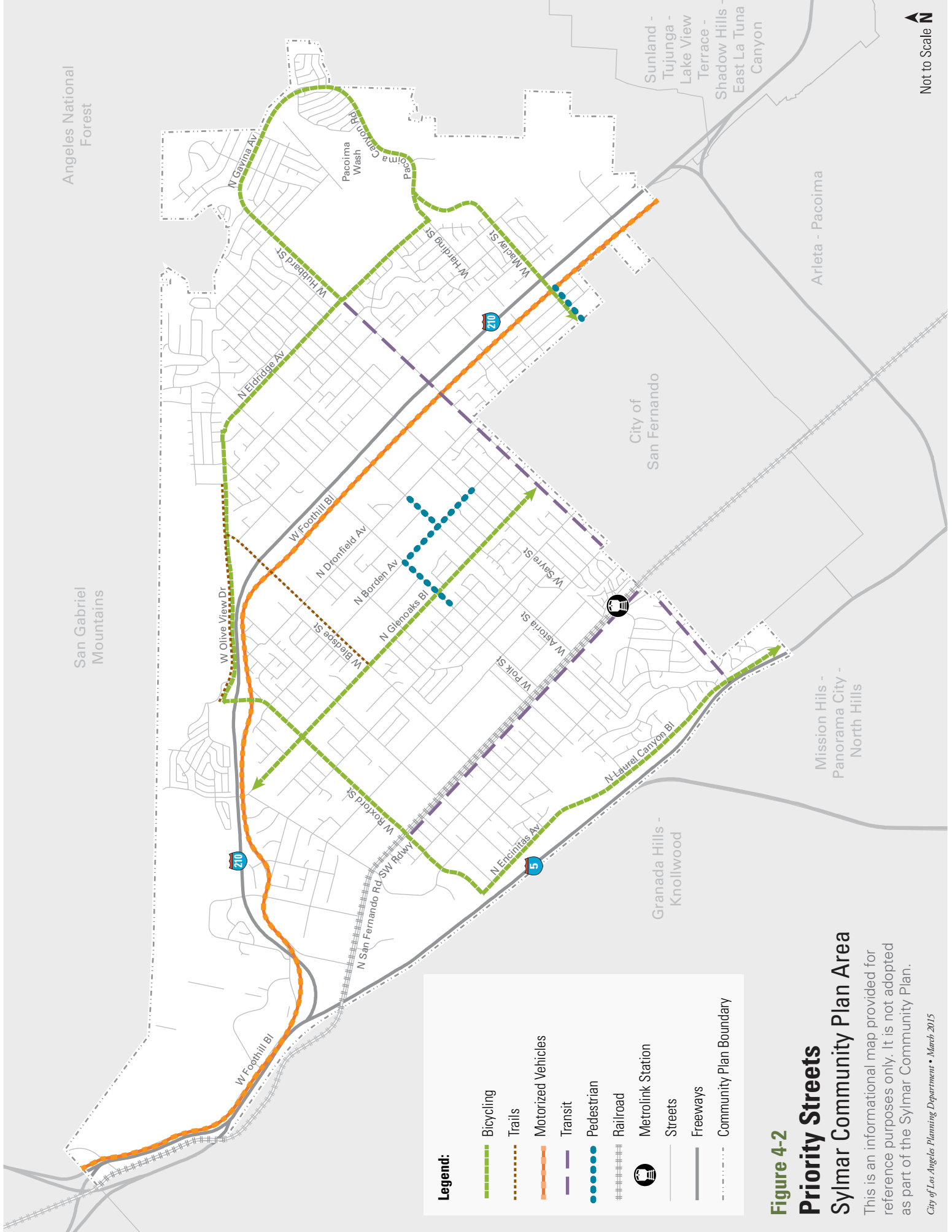
The Mobility Plan 2035 introduces the concept of prioritized improvements on the enhanced network, such as walking, bicycling, horseback riding, transit, or motor vehicle. Widening streets to accommodate additional space for every mode of travel is often unrealistic and undesirable. Instead, prioritization allows for a more tailored, efficient use of the street network that balances the needs of each mode in a holistic manner. Assigning street priorities assists City agencies, planning commissions, and elected officials in making strategic decisions about future street improvements while avoiding conflicting transportation projects. Prioritization does not preclude improvements to non-priority streets; it simply suggests where to focus attention first. Street priorities are summarized in Table 4-1, Priority Streets, and discussed further in each relevant section in this chapter.

Sylmar is a semi-rural suburban community with a street grid that was largely laid out post-World War II. Many of the streets are in need of enhancements such as sidewalks, trails, bike lanes and streetscape elements. Existing improved streets, however, have little additional land available for widening or reconfiguring to accommodate other modes. On these streets, new facilities for one mode, such as a wider sidewalk, trails, or a bicycle lane, may have to come at the expense of another, such as a travel lane for automobiles, or transit. Recognizing that all streets cannot serve all purposes, this chapter designates priority modes for certain key arterials, streets or street segments to better assist planners, engineers, developers, and the community in making these difficult choices.

TABLE 4-1

Priority Streets

Street Priority	Pedestrian	Bicycle	Trail	Public Transit	Motorized Vehicle
Astoria Street	X				
Bledsoe Street			X		
Borden Avenue	X				
Eldridge Avenue		X			
Foothill Boulevard					X
Glenoaks Boulevard		X			
Hubbard Street				X	
Laurel Canyon Boulevard		X			
Maclay Street	X	X			
Olive View Drive		X	X		
Polk Street	X				
Roxford Street		X			
San Fernando Road				X	



Legend:

- Bicycling
- Trails
- Motorized Vehicles
- Transit
- Pedestrian
- Railroad
- Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Figure 4-2
Priority Streets
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

General Mobility

The community of Sylmar is nestled in the foothills of the San Gabriel Mountains with access to major transportation systems, including bus, light and heavy rail, and a circulation system consisting of freeways, and streets. In addition, the bikeway network and Trail Network provide direct access to recreational and open space opportunities throughout Sylmar and nearby regional attractions, such as the Angeles National Forest. It is important to note that Sylmar is one of the few areas in the City where horseback riding represents both a mode of local travel and recreation for residents.

The Sylmar Community Plan recognizes the importance of developing a first-class multi-modal transportation system that minimizes impacts to the environment and neighborhoods. This Community Plan proposes to alleviate congestion primarily through reducing demand, via improvements to Sylmar's transit, bicycle, pedestrian, and trail infrastructure, however, selected signal timing and intersection improvements are also suggested. In addition, Community Plan policies in Chapter Three, Land Use and Design, support pedestrian-friendly shopping and employment opportunities, especially along Sylmar's mixed-use boulevards and transit-oriented district to encourage visitors to park once and walk store to store.

The Sylmar Community Plan seeks to foster community sustainability and livability objectives by enhancing the pedestrian-oriented commercial areas, expanding the bicycle and Trail Networks, and re-designating the classification and function of certain streets. Specifically, the Community Plan proposes to:

- Create a more pedestrian-oriented mixed-use area near the Metrolink Station, Sylmar's transit-oriented district, in order to encourage a more walkable community.
- Enhance existing trails and create new trails and trail linkages for equestrian and pedestrian use that provide enjoyable and safe travel.
- Expand the bicycle facilities and create an uninterrupted bicycle network for riders of all abilities that provide access to local destinations as well as other amenities throughout the region.
- Enhance the pedestrian environment and improve the physical conditions, making the streets more comfortable and safe for pedestrians.
- Protect neighborhoods by discouraging traffic from intruding into community's single-family neighborhoods.

Overarching Goals and Policies

Goal M1: **A diverse system of streets that balances the needs of pedestrians, bicyclists, equestrians, transit users, mobility-challenged persons and vehicles while providing sufficient mobility and abundant access options for the existing and future users of the street system.**

Policies

- M1.1 **Complete streets.** Ensure the community is served by a complete street system with some streets strategically prioritized for target user(s) and other streets that connect the arterials to serve all users, as shown in Table 4-1, Priority Streets.
- M1.2 **Mobility for Challenged Users.** Support, wherever feasible, transportation programs and services aimed at enhancing the mobility of senior citizens, disabled persons and the transit-dependent population.
- M1.3 **Mobility Enhancements.** Design developments that increase density or intensity by zone change, variance, conditional use permit, parcel map, subdivision or other discretionary action to provide adequate mobility enhancements such as traffic mitigation, pedestrian crosswalks, trails, bicycle lanes and enhanced bus stops to ensure that mobility needs are met.
- M1.4 **Private Investment for Off-site Facilities/Amenities.** Encourage new developments to include bicycle, pedestrian, and equestrian amenities and include off-site public transit and road improvements creating a circulation system that optimizes travel by all modes.
- M1.5 **Modified Street Standards.** Where there is evidence of physical or other constraints, or uses such as a transit station, the City should consider modified street standards to implement modal priorities to enhance neighborhood character and to facilitate a complete street network, as shown in Table 4-2, Street Modifications and Figure 4-3, Modified Streets and Standards.

TABLE 4-2
Street Modifications

Street Name	Specific Location	Existing Designation	New Designation	Objective
Bledsoe Street	Glenoaks Boulevard to Olive View Drive	Avenue II	Modified Local Streets	Preserve existing conditions of one travel in each direction and yield space for a trail on the south side between Glenoaks and Foothill Boulevard. Yield space for a trail on each side north of Foothill Boulevard.
Eldridge Avenue	Cranston Avenue to Hubbard Street	Avenue II	Modified Avenue II	Generally preserve existing conditions of one to two travel lanes in each direction and yield space for a designated Bicycle-Friendly street from Cranston Avenue to Sayre Street and a Bicycle Lane from Sayre Street to Hubbard Street.
Roxford Street	Telfair Avenue to Foothill Boulevard	Boulevard II	Modified Avenue I	Accommodate two travel lanes in each direction and yield space for a designated (Class II) Bicycle Lane.
Olive View Drive	Roxford Street to Kopany Avenue	Avenue II	Modified Avenue II	Preserve existing conditions of two travel lanes in each direction and yield space for a designated Neighborhood Enhanced Network Street and a trail on each side.

Avenue I & II, and Local Street Designation Standards



Figure 4-3

Modified Streets and Standards Sylmar Community Plan Area

Not to Scale 

Goal M2: **A multi-modal circulation system that supports successful neighborhood commercial areas by providing transit stations, commercial and recreational uses, with streets that accommodate public open space and gathering places, and streets that enhance sustainable watershed management.**

Policies

M2.1 **Streetscapes.** Encourage and support streetscape improvements in neighborhood areas that foster the appeal of the street as a gathering place including street furniture, well-maintained street trees and landscaping, publicly accessible courtyards, wide sidewalks, bicycle access and appropriate traffic control measures to reduce travel speeds.

M2.2 **Streetscape Plans.** Support the development of Streetscape Plans for the following streets:

- San Fernando Road between Polk Street and Hubbard Street
- Maclay Avenue between City Boundary and Foothill Boulevard
- Foothill Boulevard between Astoria Street and Hubbard Street
- Polk Street between De Garmo Avenue and Dronfield Avenue

M2.3 **Special Events.** Encourage and support special street closures for community activities such as street fairs, parades, festivals and other civic events. Encourage the temporary closure of local and collector streets for the purposes of providing space for farmer’s markets, where appropriate.

M2.4 **Watershed Management.** Support watershed management in the design of streets, particularly streets near the Pacoima Wash, by incorporating swales, water retention and other such features in new development, such as streetscape programs and other street improvement programs.



The Sylmar Farmer's Market located on the campus of LA Mission College.



The arroyo streambed that runs through LA Mission College captures rainwater onsite.

Goal M3: **A system of safe, efficient, and attractive pedestrian, bicycle and trail facilities linking neighborhoods to key areas in the community, including commercial centers, services and employment, points of historical interest, as well as open space and recreational areas.**

Policies

- M3.1 **Safety for All Users.** Minimize conflicts between the various modes of motorized and non-motorized transportation by designing and constructing roads, sidewalks, crosswalks, bicycle lanes and trails to their proper specifications with appropriate signage and well-marked crossings to ensure safety for all users of the roadway, including buses, cars, pedestrians, bicyclists, and equestrians.
- M3.2 **Safe School Routes.** Encourage the development and improvement of safe routes to schools throughout the community, particularly along Polk Street, Borden Avenue, and Astoria Street, via walking, bicycling, or by public transit.
- M3.3 **Easements and Right-of-Ways.** Encourage the safe utilization of easements and/or right-of-ways along the Pacoima Wash, flood control channels, utilities, railroad right-of-ways and streets wherever feasible for pedestrians, bicyclist, and equestrians.
- M3.4 **Underutilized Right-of-Ways.** Repurpose underutilized roadways and right-of-ways for recreational uses.
- M3.5 **Reclaimed Land.** Incorporate trails and bicycle facilities into recreational reuse of reclaimed land such as of utility right-of-ways, flood control channels, and access roads.



Flood control channels in Sylmar provide opportunities for hiking and trail connections.



Potential trails and bicycle lanes along the Pacoima Wash. Rendering courtesy of Pacoima Beautiful.

Walking

The benefits of walking as a mode of transportation are vast, including a healthier community, more social interaction, better air quality, a reduced carbon footprint, and substantial cost savings on automobile-related expenses. In 2008, the City Planning Commission adopted the “Walkability Checklist,” which instructs developers, architects, community members, and decision makers to design projects that encourage pedestrian activity.

The pedestrian network in Sylmar consists primarily of sidewalks provided along most roadways in the community. Sidewalks vary in width and physical conditions, making some more attractive to walking than others, offering different levels of walkability. Many factors affect walkability in Sylmar, including physical barriers such as railroad tracks, freeways, and limited pedestrian crossings. Although these barriers discourage or in some places prohibit pedestrian access and limit pedestrian connectivity between many neighborhoods, the community of Sylmar is still very much active. Most of the existing pedestrian activity in Sylmar is concentrated around and along school routes, in particular Olive Vista Middle School and Sylmar High School which are located adjacent to the Sylmar Park Recreation Center between Polk and Astoria Streets. The areas near the Sylmar/San Fernando Metrolink Station and Mission College and El Cariso Community Regional Park also experience high volumes of pedestrian activity. The Pacoima Wash and nearby trails attract recreational and exercise-oriented pedestrian traffic, particularly on weekends and during the summer, while the shopping centers attract pedestrians for business and leisure activity, such as dining and other entertainment.

Enhancing the pedestrian environment has been cited as particularly important to the community. Desired enhancements that improve the physical conditions and make the streets in Sylmar more comfortable and safe include wider sidewalks, street trees, and pedestrian crosswalks. Improving connectivity between areas and various modes of transportation is also of importance, especially along San Fernando Road, Foothill Boulevard, and Maclay and Polk Streets where housing density, non-commercial activity, and transit use are more common and conflicts more likely. This Community Plan includes policies for increasing opportunities to walk, as both a means of transportation and recreation, within the Sylmar community. Several streets are identified as pedestrian oriented areas and support the development of a “main street” design that emphasizes pedestrian over vehicle circulation, as shown in Figure 4-2, Priority Streets.

Walkability Checklist

The “Walkability Checklist” provides a list of recommended strategies that projects should employ to improve the pedestrian environment in the public right-of-ways and on private property. Each of the implementation strategies on the Checklist should be considered in a proposed project, although not all will be appropriate in every proposed project. Each project will require a unique approach. While the checklist is neither a requirement nor part of the zoning code, it provides a guide for consistency relating with the policies contained in the General Plan Framework. Incorporating these guidelines into a project’s design will encourage pedestrian activity, more appropriate forms, and placemaking. A project that is walkable is good for business and the environment.

Pedestrian Priority Streets

Pedestrian Priority Streets are identified streets within districts where pedestrian activity is encouraged, including neighborhood centers, community and regional commercial centers, and areas adjacent to schools and other public facilities.

Improvements for these streets include sidewalks wide enough to include ample pedestrian amenities such as kiosks, street benches, bus shelters, planters and pedestrian signage and lighting. Building frontages should provide a high level of pedestrian interest. Pedestrian crossings should have a high priority at intersections. In some locations, well-protected mid-block crosswalks or bulb-outs may be appropriate.

Goal M4: A community-wide, pleasant street environment that is universally accessible, safe, and convenient for pedestrians.

Policies

- M4.1 **Pedestrian-Oriented Development.** Encourage walking by orienting building entrances to face the streets and sidewalks when designing new developments and buildings. Promote well-designed retail with transparent facades to allow visibility of commercial uses and encourage large commercial projects to consider designs which break up the floor plate, providing pedestrian connections, and human scale design features, such as plazas, green spaces or a public focal point. Discourage “superblocks”. Refer to Chapter 3, Land Use and Urban Design, for additional policies.
- M4.2 **Pedestrian Priority Streets.** Streets within commercial, mixed-use and employment districts should have pedestrian priority establishing pedestrian needs as paramount to vehicular circulation needs. Investment in pedestrian improvements and programs for these segments should be encouraged, as shown in Figure 4-2, Priority Streets.
- M4.3 **Pedestrian Amenities.** Maintain sidewalks, streets and right-of-ways in good condition, free of obstructions, and with adequate lighting, trees and parkways. Streets must accommodate pedestrians comfortably through adequate sidewalks and parkway landscaping that provides a buffer from moving vehicles, shade from the sun, and street lighting that provides for safety at night, unless specifically prescribed by the community for trails, equestrian amenities, or rural aesthetics.



Buildings with awnings and street trees provide shade and weather protection making streets comfortable for pedestrians.



Landscaping buffers with trees and pedestrian-scale lighting and signage provide a comfortable, inviting pedestrian environment.

Bicycling

Los Angeles is an ideal position to encourage bicycle usage. Excellent climatic conditions for bicycling in Southern California prevail approximately 340 days per year. By increasing the number of bicyclists who ride for commuting and other utilitarian purposes, traffic congestion is reduced and air quality is improved. In addition, bicyclists benefit from improved health and fitness. A large portion of personal trips are two miles or shorter, which people may prefer to complete by bike, if a safe route exists.

The community of Sylmar has many features that make bicycling pleasurable: a mild climate, relatively flat terrain, and proximity to vast open space and local and regional trails. The bikeway system in Sylmar consists of a network of bicycle lanes, neighborhood enhanced network-streets, and paths. This Community Plan prioritizes certain streets for bikeway and streetscape improvements, improving non-motorized access to educational facilities in the area, public transit, commercial areas, and employment centers. It establishes policies to facilitate the development of bikeways and facilities that support bicycling, implementing the City's Mobility Plan, which is intended to enhance and prioritize the connection to other modes of public transportation and non-motorized alternatives and increase mobility options and recreation opportunities, as shown in Figure 4-2, Priority Streets, and Figure 4-4, Bicycle Plan.

Citywide Bicycle Plan

The City's 2010 Bicycle Plan, a part of the Mobility Plan 2035, was created to enhance bicycle transportation at a citywide scale and includes three goals: (1) to increase the number and types of bicyclists who bicycle in the City, (2) to make every street a safe place to ride a bicycle, and (3) to make the City of Los Angeles a bicycle-friendly community. The Mobility Plan 2035 is a comprehensive revision of the Transportation Element, It's consistent with these goals. Specifically, the Mobility 2035 calls for increased bikeways along Boulevard II Streets, particularly those with Rapid bus service, as well as the establishment of Neighborhood Enhanced Network Streets on streets with low traffic volumes and slow speeds.



Wide outside travel lanes provide adequate on-street space for bicycles and vehicles to share the lane without requiring the vehicle to leave its lane to pass the bicycle.



Well marked bicycle lanes and paths are important for travel safety and reduce conflicts between bicyclists and automobiles.



Bicycle path.



Bicycle lane.



Bicycle route.



*Neighborhood street. Creative Commons.
Courtesy of Peyton Chung*

Bikeway Standards

A “bikeway” is a generic term for any road, street, path or way that in some manner is specifically designed for bicycle travel, regardless of whether such facilities are designated for the exclusive use of bicycles or are to be shared with other transportation modes. The Federal and state transportation system recognizes three primary facilities: Bicycle Paths (Class I), Bicycle Lanes (Class II), and Bicycle Routes (Class III). The City has also established a new Neighborhood Enhanced Network Street classification. See the following for descriptions.

- **Bicycle Path** (Class I Bikeway). A paved pathway separated from motorized vehicular traffic by an open space or barrier, and either within the roadway right-of-way, or within independent alignment. Bicycle paths may be used by bicyclists, skaters, wheelchair users, joggers, and other non-motorized users.
- **Bicycle Lane** (Class II Bikeway). Bicycle lanes dedicate a portion of the roadway for preferential or exclusive use by bicyclists through striping, signage, and pavement markings.
- **Bicycle** (Class III Bikeway). A shared roadway for use by bicyclists, intended for streets with lower traffic volumes and speeds, usually with wide outside lanes, signalized intersections at crossings and/or cross-street priority, denoted by signs only.
- **Neighborhood Streets**. (Class III Bikeway). Neighborhood streets are a new type of Bicycle Facility established in the Mobility Element that gives bicyclists expanded access (via local and collector streets) with limited motor vehicle through-traffic, lower speeds, and various design elements to enhance bicycle safety and enjoyment.
- **Protected Bicycle Lane/ Cycle Track** (Class IV Bicycle lanes that provide further Bikeway). protection from other travel lanes by the use of a physical roadway intervention.

Goal M5: **A safe, comprehensive, and integrated bikeway network that is accessible to all, and encourages bicycling for recreation and transportation.**

Policies

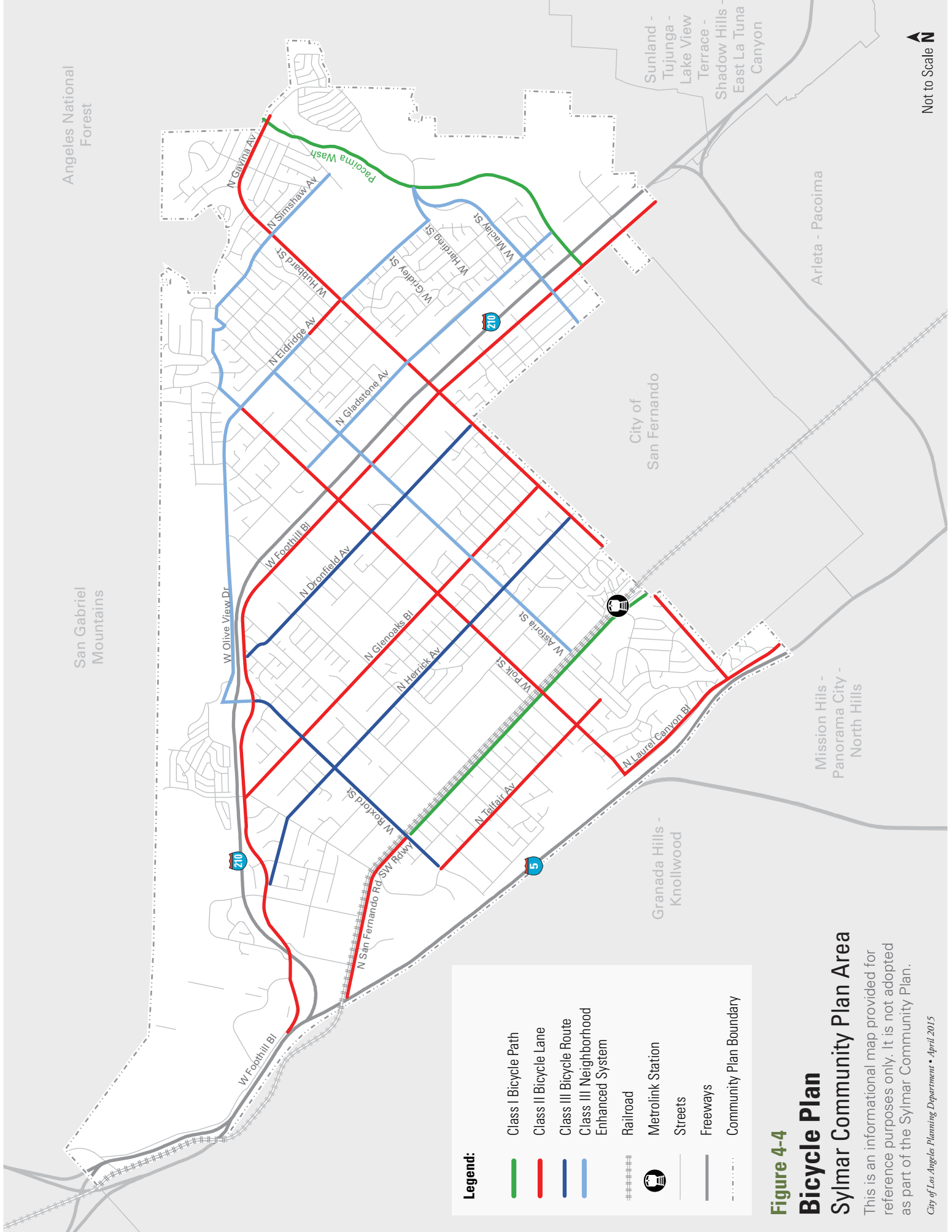
- M5.1 **Bikeway Connections.** Provide bicycle access for open space areas, mixed-use corridors, commercial corridors, neighborhood districts and community centers to allow easy connection between residential neighborhoods and employment centers, as well as important non-work destinations. Refer to Chapter 3, Land Use and Urban Design and Chapter 5, Community Facilities, Infrastructure and Open Space for additional policies.
- M5.2 **Bicycle Priority Streets.** Support the Citywide bikeway network to establish bicycle circulation as paramount to vehicular circulation needs on key streets and to encourage investment in bicycle improvements and programs on these identified streets, as shown in Figure 4-4, Bicycle Plan. Bicycle Priority Streets in Sylmar are also shown in Figure 4-2, Priority Streets.
- M5.3 **Public Improvements.** Implement public right-of-way improvements on Eldridge Avenue, Roxford Street, and Olive View Drive and restripe sections of these streets to provide two-lanes of travel and allow for bicycle lanes on both sides of the street, as shown in Figure 4-3, Modified Streets and Standards.
- M5.4 **Parking Restrictions.** Consider implementing parking restrictions along Bicycle Priority Streets, such as Glenoaks Boulevard, to accommodate bicycle improvements, where appropriate.
- M5.5 **Bicycle Amenities.** Incorporate bicycle amenities, such as parking, lockers, changing rooms and showers, in public facilities, parks, commercial developments, employment and transit centers, and park and ride facilities.
- M5.6 **Regional Coordination.** Coordinate with appropriate City and County agencies, adjacent jurisdictions, non-profit organizations and the local community to require bikeways be linked with those existing and proposed in adjacent areas.

Bicycle Priority Streets

Bicycle Priority Streets are identified streets where bicycle activity is encouraged, through increased travel connections and safety. Bicycle paths, lanes, and routes each have different characteristics from the others, but generally require space in the roadway to accommodate bicyclists safely and provide greater visibility to motorists. Improvements for these streets include separated right-of-ways, striped lanes for one-way bicycle travel on a street or highway, bicycle way signage, and amenities in the streetscape such as bicycle racks and lockers.



Bicycle racks encourage customers to ride a bike to neighborhood and commercial districts for short trips and errands.



Angeles National Forest

San Gabriel Mountains

Sunland - Tujunga - Lake View Terrace - Shadow Hills - East La Tuna Canyon

Arleta - Pacoima

City of San Fernando

Mission Hills - Panorama City - North Hills

Granada Hills - Knollwood

Not to Scale

Legend:

- Class I Bicycle Path
- Class II Bicycle Lane
- Class III Bicycle Route
- Class III Neighborhood Enhanced System
- ##### Railroad
-  Metrolink Station
- Streets
- Freeways
- - - - - Community Plan Boundary

Figure 4-4
Bicycle Plan
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Trails

Healthy and livable communities depend upon recreational opportunities as an important amenity. The circulation network both serves and can become an integrated part of the recreational opportunities available to the community. In addition to walking and bicycling for health and recreation, communities like Sylmar choose to facilitate horseback riding for relaxation and enjoyment. Planning a system of trails provides the essential outdoor resource necessary for the use of and access to natural features, including hillsides and rivers.

Sylmar is one of the few communities in the City with a network of local trails that connect neighborhoods to various points of interest, parks, recreational and natural open space areas, and other communities. These trails serve as a valuable recreational resource for area residents and visitors and are an important mode of local travel. The network in Sylmar primarily consists of undeveloped trails that share the street right-of-way with pedestrians, bicyclist and motorists. Much of the developed trails with dedicated easements and/or paths set-aside for equines and pedestrians are located on Foothill Boulevard, Olive View Drive, and along public right-of-way parallel to the Foothill Freeway. Concerns about equine and rider safety are mentioned often by community members, as are accessibility and maintenance of trails.

This Community Plan includes policies for increasing opportunities to horseback ride, as both a means of transportation and recreation, within the Sylmar community. It establishes policies to facilitate the development of the Sylmar Trail Network, which reinforces the viability of equinekeeping land uses and aims to enhance and prioritize the connection to other recreational and open space opportunities and to the City’s Major Equestrian and Hiking Trails Plan and the Rim of the Valley Trail Network, as shown in Figure 4-5, Trail Network. Further, greater attention to equestrian amenities and design can increase safety and encourage horseback riding as a recreational activity and a viable way of getting around Sylmar.



Undeveloped trails leading to open space areas represent a valuable resource for non-motorized recreational activities.

Citywide Trails Plan

City trails are unique circulation features that directly support land uses, such as equine uses, within a community. They offer a way for equestrians to move about on horseback and can serve as an alternative to the automobile.

The City’s 1968 Major Equestrian and Hiking Trails Plan established a backbone Trail Network which links various equestrian communities within the City, creating a continuous loop around the northern half of the City and San Fernando Valley. The Trails Plan has served as a guide for decision makers concerned with the development of existing and proposed trails and general location of trail stops, equestrian centers, and equinekeeping districts within the City. It also established recommendations for trails outside of the City.

The 1990 Santa Monica Mountains Conservancy’s Rim of the Valley Trail Corridor Master Plan created an interlocking, connected system of parks, trails, and wildlife habitat preserves within the foothills and mountain areas encircling the San Fernando and La Crescenta Valleys. This Rim of the Valley Trail, which connects to Sylmar, is a recreational trail open for use by hikers and equestrians.



North Arrow
Not to Scale

Legend:

- Trail
- Equestrian Crossing
- Railroad
- Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Figure 4-5
Trail Network
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted part of the Sylmar Community Plan.

Goal M6: **A trail network that meets the needs of residents by providing scenic and enjoyable experiences that include connections with other public facilities, such as parks and recreational areas, open spaces, the regional trails system, points of interest, and sites with educational or historical significance.**

Policy

- M6.1 **Trail Network.** Protect and expand the Trail Network in Sylmar which reinforces the viability of equine uses and accessibility to open spaces and recreational opportunities by requiring new development, subdivision tracts, parcel maps, small lot subdivisions, and infrastructure improvement projects which abut or connect with a trail to develop and/or improve the Trail Network, as shown in Figure 4-5, Trail Network, and according to goals, policies, and design guidelines in Chapter 3, Land Use and Urban Design.
- M6.2 **Trail Priority Streets.** Support the Trail Network to establish equestrian circulation as paramount to vehicular circulation needs on key streets in Sylmar and to encourage investment in trail improvements and programs on these identified streets. Trail Priority Streets are shown in Figure 4-2, Priority Streets.
- M6.3 **Public Improvements.** Implement right-of-way improvements on Bledsoe Street between Glenoaks Boulevard and Olive View Drive to maintain existing roadway conditions and allow for a trail on both sides of the street, as shown in Figure 4-3, Modified Streets and Standards.
- M6.4 **Trail Connections.** For projects over which a trail is designated or existing (crosses through the project site), as shown on the Trail Network map, refer to Chapter 3 goals, policies, and design guidelines, and consider providing access to trails not provided by other dedicated public trails in the vicinity, connecting to existing dedicated public trails, and providing a trail linkage from the project to existing dedicated public trails. Trails may be located within developments and/or on the periphery of developments and the course of the trail may be altered to maximize land use as long as the altered course is safe and maintains connections and continuity to the Trail Network and other trails. If the course of the trail changes, subsequent projects on neighboring lots should continue the same course.
- M6.5 **Recreational Trails.** Maintain, improve, and/or develop recreational trails in open space areas, agricultural land, and utility and right-of-ways which link residents to parks, open space, public facilities, and other trails and create healthier and more sustainable communities. Encourage where appropriate a network of trails to facilitate recreational use such as horseback riding and hiking. Refer to Chapter 5, Community Facilities, Infrastructure, and Open Space for additional policies.

Trail Priority Streets

Street improvements for equestrian priority streets may include dedicated easements and trails designed specifically for public use by equestrians, rider-height traffic signals, crosswalks, and other traffic management techniques to facilitate safety and avoid conflicts. Improvements for equestrians also serve a dual purpose for pedestrians, providing safe walking areas.



Trails that incorporate landscaping complement the visual character of the recreational resource.



Trail policies and design guidelines provide details on trail easements and development which contribute to the equestrian lifestyle in Sylmar.



When trails intersect roads, safety is the most important factor. Installing a second push button signal actuator allows users to control the traffic light and safely cross the road.

M6.6 **Trail Amenities.** Incorporate trail amenities that support the equestrian lifestyle, such as hitching, parking, and staging areas, that are adjacent to or near the Trail Network. Where appropriate, provide recreational trails, a turnout, vista points and other complementary facilities.

M6.7 **Trail Safety.** Where trails are identified along arterial roadways, incorporate appropriate safety measures, such as signage, crosswalks, equestrian waiting areas, and rider-height signal actuator buttons at signalized intersections, to accommodate equestrian use and minimize conflicts between equestrians, pedestrians, bicyclists and automobiles. Streets where equestrian safety measures are recommended include Olive View Drive and Bledsoe Street, Gladstone Avenue and Polk Street, and Glenoaks Boulevard and Cobalt Street. Refer to Figure 4-5, Trail Network.

M6.8 **Regional Coordination on Public Land.** Coordinate with the necessary public agencies to acquire rights to improve the easements along the flood control channels and the high-tension transmission lines and areas of public open space, to be improved and developed as trails and linked with those of neighboring areas.

M6.9 **Character and Design.** Develop trails and amenities according to the guidelines in the Design Guidelines for Trails.



A trail located at the rear of a commercial property, leading up to the Angeles National Forest, separated and away from vehicular traffic.



A shared-use, single tread trail with double track (two lanes) work well when all user groups are compatible, such as pedestrians and horses. Potential trail along the Pacoima Wash. Rendering courtesy of Pacoima Beautiful.

Scenic Highways

Goal LU25: In addition to the built form and streetscape in Sylmar, natural and man-made open space amenities help define the community. The San Gabriel Mountains to the north and the Pacoima Wash which traverses the southeastern section of the community are among the most prominent open space features. The Lakeside Debris Basin and the Angeles National Forest adjacent to the northern section of the community add to the richness of Sylmar’s natural landscape.

Goal LU26: The value of scenic vistas must be considered in planning for community accessibility to natural areas. Both freeways which traverse through the community (I-5 and I-210) are designated as Scenic Highways and are on the City’s Scenic Highways Plan, as shown in Figure 4-1, Circulation System. They afford views of the San Gabriel Mountains, the Santa Susana Mountains, and the San Fernando Valley. The preservation and protection of these two scenic corridors should be an integral part of the design of buildings and structures that are constructed adjacent to or near freeways in order to maintain their existing panoramic scenic views.

Goal M7: **A community with abundant opportunities for exploration of its natural assets and a circulation system that enhances the quality of life and aesthetic value of the area.**

Policy

- M7.1 **Scenic Highways.** Continue to preserve existing scenic highways and byways and support programs to encourage the identification of additional scenic highways and/or byways within the community. Scenic Highways are shown in Figure 4-1, Circulation System.
- M7.2 **Development.** Require development, including signs, adjacent to a Scenic Highway to protect public views of scenic vistas to the maximum extent feasible; be adequately landscaped to soften the visual impact of the development, restrict height, and establish design controls in order to maintain the integrity of scenic highways within Sylmar. Refer to Chapter 3, Land Use and Urban Design, and Chapter 5, Community Facilities, Infrastructure, and Open Space for additional policies and design guidelines.

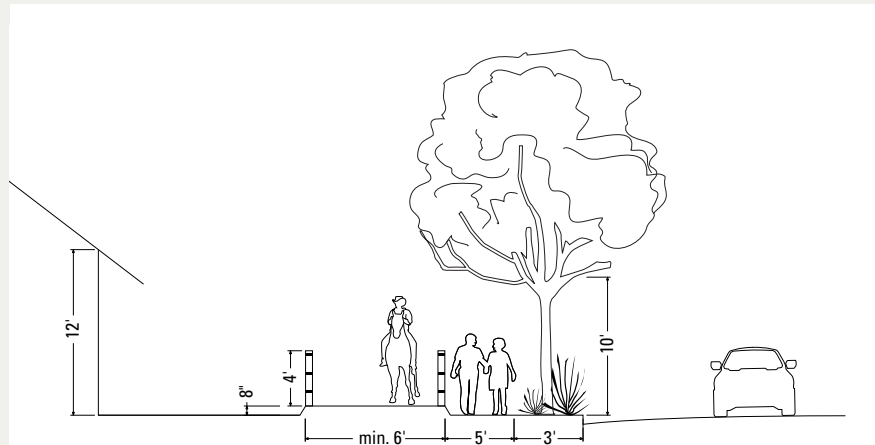


The Pacoima Wash, located adjacent to the Angeles National Forest, provides large open space areas, hiking, and equestrian trails with spectacular scenic vistas.



The northwest portion of Sylmar features equestrian trails that connect with the Rim of the Valley, with mountain views to the north.

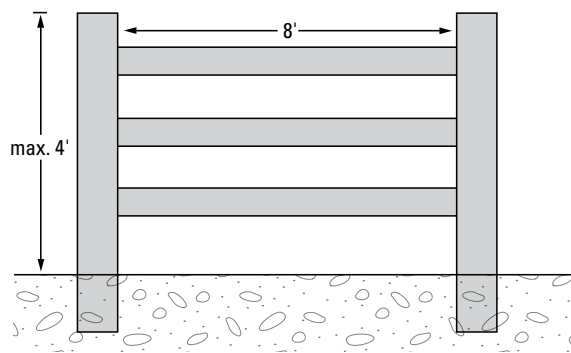
Design Guidelines for Trails



See G1 - G3

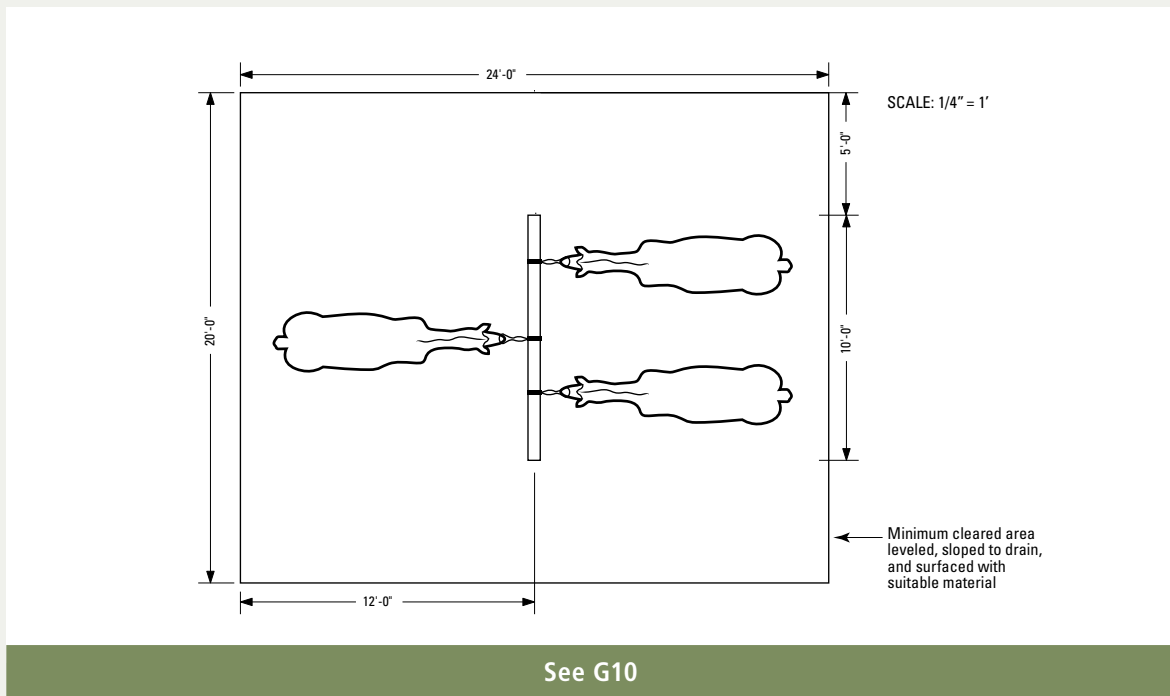
- G1. Trails adjacent to streets should be designed to be a minimum of 6 feet in width to accommodate a single track. Preferred trail width adjacent to streets is between 10 and 12 feet to accommodate a double track.
- G2. A vertical clearance of 10 feet should be maintained from the ground and any physical barrier such as bridges, underpasses, and should maintain vegetation free of protruding branches.
- G3. A maximum height of 4 feet is recommended for all fences and barriers along trails. A greater height may be permitted for trails adjacent to high speed roads where traffic may startle horses. Height should be tapered down as trail approaches intersections or end, to maximize horse/ rider view.
- G4. Low walls or fences with railings added for more height are acceptable. Bollards, barrier posts, or rail tie "stopovers" at forest/mountain trail head can help separate equestrian from other uses. Barrier posts should be an odd number to prevent confusion, and placed 5 feet apart to allow equestrians to pass through.

Design Guidelines for Trails (continued)



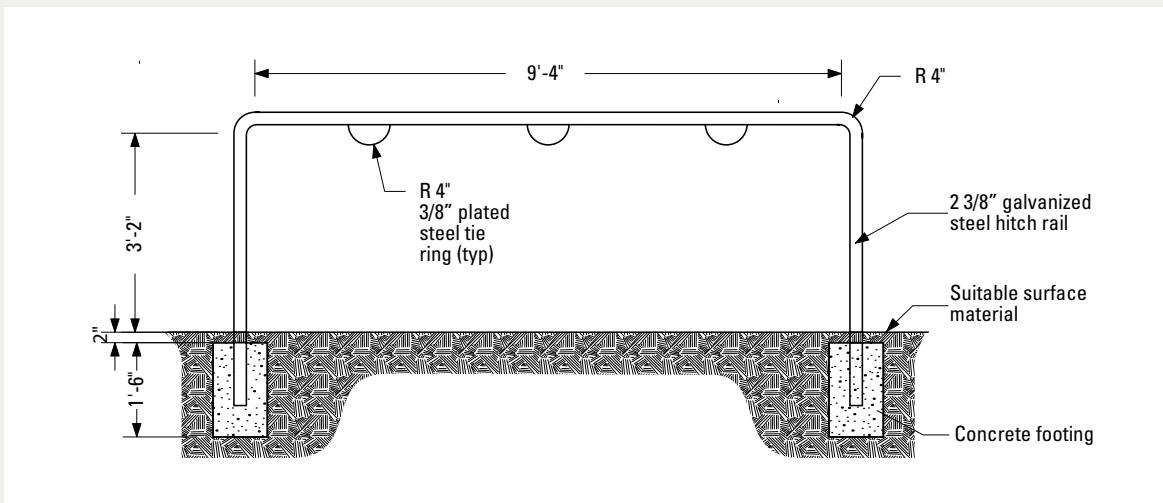
- G5. G5 Preferred fence materials include any sturdy material that gives the appearance of wood-like finish.
- G6. Use of native plants for landscaping is encouraged. Low walls or fences can include vegetation facing the trail to improve appearance, especially along trails with pipe railing. Vegetation should be trimmed to less than 4 feet high for crime prevention purposes, and trimmed to avoid injury to equines. Plants toxic to equines, such as oleander, must be removed.
- G7. Trails adjacent to or within 6 feet of ditches or steep slopes that rise more than one foot in 3 feet and have drop off of over 2.5 feet should begin at least 8 feet before and extend 8 feet beyond the vertical hazard. A minimum 3 foot shoulder from the rail edge to the trail should be provided with an 8-inch maximum spacing between rails.

Design Guidelines for Trails (continued)



- G8. Trail treads should be a fine aggregate material such as decomposed granite or other non-slip, porous surface.
- G9. Trail easements/improvements that are adjacent to commercial and industrial sites should be located away from vehicular traffic, such as along the rear of the site, for safety.
- G10. Equine parking or hitching area separate from vehicle parking should be a minimum of 20 feet X 24 feet, leveled, sloped to drain, constructed of permeable surface, and maintained clear of vegetation and objects. Hitching areas should include a hitching rail located in the center of the hitching pad area and be clear of vehicular traffic. Minimum clearances should be observed.

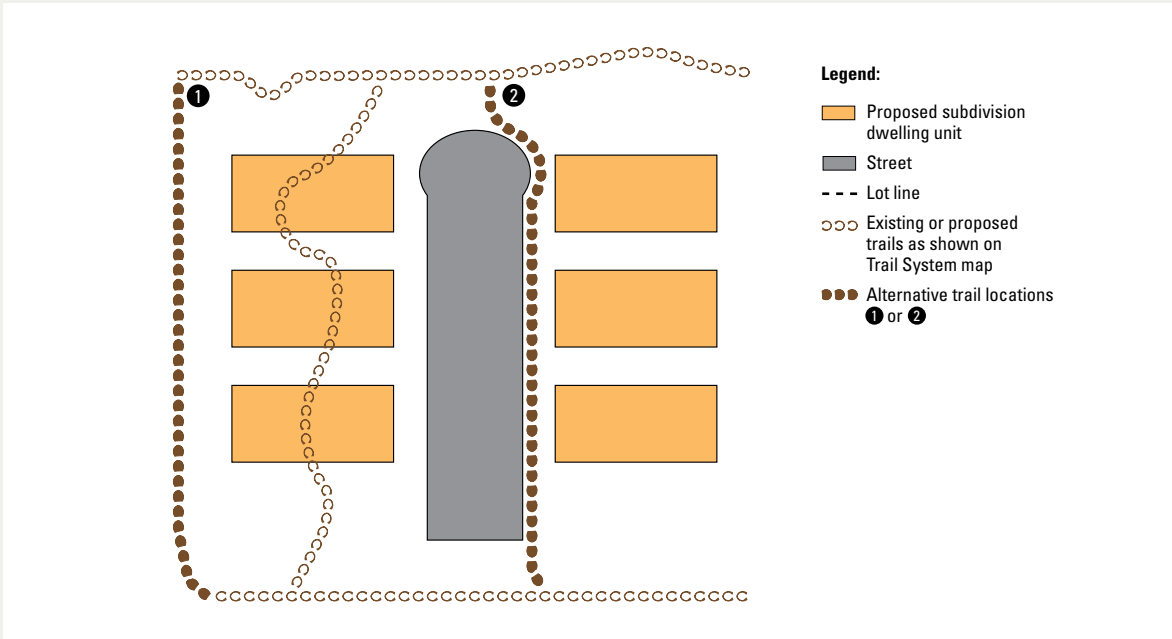
Design Guidelines for Trails (continued)



See G11-G12

- G11. The hitching rail(s) should accommodate small riding groups, be of durable material, finished with rounded edges and no overhanging ends, to avoid injuries, with welded loops or braces in the corners to confine lead ropes and prevent slippage. The hitching rails length should be a minimum of 4 feet with a height of 42 inches.
- G12. Solid metal hitching loops should be set into brick or concrete wall surrounding a vehicle parking lot to accommodate equestrian parking in existing or new lots. A soft surface is preferable, with a minimum 24-foot distance to the nearest vehicle parking space.
- G13. A second signal actuator push button, and equine crossing signal, 5 to 6 feet above the ground should be installed. The post should be set 6.5 feet from the road edge so that the animal's head does not encroach into the roadway. The equestrian waiting area at the signalized crossing should be 25 feet X 15 feet.
- G14. The equestrian waiting area at a signalized crossing should include a landscaped median for horse refuge.

Design Guidelines for Trails (continued)



See G15

G15. The course of the trail should be altered to maximize land use as long as the altered course is safe and maintains connections and continuity to the Trail Network in Sylmar and other trails. Design Guidelines for Trails.

Public Transit

Public transit, including high-speed and commuter rail, subways, light rail, street car, bus rapid transit, and express and local buses, is a crucial component of the City's transportation system and is the most efficient means of moving people throughout the region. Transit accessibility increases mobility by providing people with expanded options for commuting to and from school, work, shopping areas, entertainment parks, beaches, and other activities. It also provides an important service to those without access to a car, either by choice or due to age, ability, or income. Transit riders save money and produce fewer greenhouse gases than their driving counterparts.

Sylmar is served by local Metro transit routes that run on key arterial streets (Boulevard or Avenues), a number of LADOT commuter express routes serving major employment centers throughout the City, the Metrolink commuter rail line from Lancaster to Downtown Los Angeles, as well as other municipal and regional bus routes, including the Santa Clarita and Antelope Valley Transit routes. While public transit is provided and maintained by other agencies, land use and zoning decisions, connectivity to other modes, including walking and bicycling, and improving traffic operations within key corridors to facilitate bus headways, can greatly influence ridership. This Community Plan includes policies which encourages transit-oriented development near the Sylmar/San Fernando Metrolink Station and in major economic activity areas to accommodate growth and reduce the need for driving. It also encourages development and public improvements at key intersections along transit routes to promote safer and more convenient access to public transit. This Community Plan identifies several transit priority streets, as shown in Figure 4-2, Priority Streets.



Public transit connections to key neighborhood districts, as well as areas of employment, education and recreation, provide expanded mobility options for residents and visitors of the community. Bus stop in front of LA Mission College.



Sylmar's Metrolink Station serves as a multi-modal transit terminal and transfer point for users of public transit.

High-Speed Rail

High-speed rail is a statewide initiative to supplement air travel by providing rail connections between northern, central and southern California. The High-Speed Rail Authority is currently envisioning high-speed rail to traverse the Sylmar community. For safety and efficiency, high-speed rail requires complete grade separation of rail and surface streets; such grade separation will have an impact on the existing semi-rural suburban environment.

Goal M8: An integrated land use and public transit strategy that directs growth to areas which are accessible by public transit facilities and services.

Policies

- M8.1 **Transit Connections to Key Areas.** Increase public transit access to neighborhood districts, community centers and mixed use boulevards. Coordinate with Metro and the Department of Transportation to improve local, Metro Rapid, and community-level bus service.
- M8.2 **Development at Transit Nodes.** Facilitate development and public improvements at the Sylmar/San Fernando Metrolink station and intersections that Metro identifies as major transfer nodes to promote convenient access between new development and the public transit system. Refer to Chapter 3, Land Use and Urban Design, for additional policies.
- M8.3 **Private Transit.** Encourage new major developments to provide on-demand shuttle services to the Sylmar/San Fernando Metrolink Station, local Metro bus stops, community centers, mixed use boulevards, and other destinations within the community.



Bicycle lockers provide needed security and encourage bicyclists to utilize public transit.



Metrolink commuter rail service provides service from Lancaster to Sylmar/San Fernando, allowing residents in Sylmar to connect to other public transit services. Photo Credit: Charles Freericks

Goal M9: **An expanded public transit system that provides residents, employees, and visitors safe and efficient access to jobs, services, recreation and other community assets so that automobile dependence may be reduced.**

Policies

- M9.1 **Transit Priority Streets.** Support the identification of transit priority street segments with high transit vehicle volumes to facilitate public transit circulation as paramount to vehicular circulation needs and to encourage investment in transit improvement programs for the identified routes. Transit Priority Streets are shown in Figure 4-2, Priority Streets.
- M9.2 **Transit Improvements.** Support the development of multi-modal transit plazas on San Fernando Road and Glenoaks Boulevard, near Hubbard Street. The closure of Truman Avenue is recommended in order to create a public plaza and improve transit connections and access.
- M9.3 **Transit Access and Amenities.** Provide enhanced amenities at major transit stops, including widened sidewalks, where possible, pedestrian waiting areas, transit shelters, enhanced lighting, improved crosswalks, information kiosks, and advanced fare collection mechanisms, shade trees, bicycle access and self-cleaning restrooms. Improve the ease and convenience of using transit by making improvements to transit waiting areas and pedestrian and bicycle facilities leading to transit waiting areas.
- M9.4 **Street Enhancements for Buses.** Support street improvements, particularly along Hubbard Street, which are needed to facilitate the movement of buses, such as jog eliminations, street widening, bus bays or turnouts, street signage, striping, and colored pavement.
- M9.5 **Express Bus Focus.** Connect express bus service, such as DASH, Commuter Express, Metro Rapid and Bus Rapid Transit, to transit centers and park and ride facilities to key destinations within Sylmar, including the Bradley industrial area, shopping centers, Mission College, and local parks.
- M9.6 **High-Speed Rail.** Conduct studies to determine the appropriate uses and amenities necessary to increase ridership, while balancing the needs of the community, if a high-speed rail station or line is planned for Sylmar.

Transit Priority Streets

Transit priority streets are arterials where bus use is prioritized. The design of these streets should support the comfortable use of transit, utilizing wide sidewalks, landscaping, attractive street furniture and well designed bus stops/shelters. Pedestrian amenities, such as trash cans and benches, and safety measures, such as pedestrian lighting and special crosswalk paving, help support a pedestrian friendly environment along these streets. Roadway construction features should include concrete bus pads, bus only curb lanes, and other features to address the extra maintenance issues associated with high volumes of bus traffic.



Sheltered bus stops protect passengers from the elements and provide seating while waiting for a bus.

Motorized Vehicles and Green House Gas Emissions

Gasoline and diesel powered motor vehicles contribute significantly to green house gas emissions equaling increased localized air pollution and resulting in long-term climate change. According to the California Air Resources Board, 2006 Greenhouse Gas Inventory, tail-pipe emissions from motor vehicles accounted for 35.3% of the green house gas emissions in California. Reducing the number of vehicle trips (trips) and the length of vehicle trips (vehicle miles of travel, or VMT) becomes an important sustainability goal for residents' health and quality of life.

Motorized Vehicles

Motorized vehicles include cars, trucks, motorcycles, and scooters, and are the primary mode of transportation for the majority of the City's population. The prevalence of vehicular traffic on our streets at times has negatively affected the quality of life in Sylmar's residential neighborhoods and along commercial streets. Increased levels of vehicular congestion and extended peak hour traffic periods have decreased each individual's mobility and access to goods and services. Emergency vehicle access, which may be impacted by congestion and an incomplete street system, is also a concern within the community, particularly in the foothill areas.

Sylmar's circulation system serves the local community well but breaks down during morning and afternoon rush hours due to commuter congestion on the boulevards, on the on- and off-ramps of the I-5 and I-210 freeways, and railroad crossings. Congestion was mentioned as a principal mobility concern, particularly on Hubbard Street near Los Angeles Mission College, on Roxford Street at the entrance of the I-5 freeway, Foothill Boulevard at the entrance of the I-210 freeway, and around the schools. This Community Plan proposes to alleviate congestion through improvements to non-motorized transportation infrastructure, signalized timing, and intersection improvements. In addition, Community Plan policies in Chapter 3, Land Use and Urban Design, support the development of more shopping and employment opportunities within walking or biking distance from many of the community's residential areas, making a more walkable community.



Glenoaks Boulevard, a major thoroughfare in Sylmar.

Goal M10: **A network of streets, , and freeways that supports existing and planned land uses and provides improved motorized vehicle mobility throughout Sylmar, particularly in congested corridors.**

Policies

- M10.1 **Priorities for Capacity Enhancements.** Implement a safe and efficient transportation network, and increase its capacity through, in priority order, the provision of alternative transit options, transportation demand management (TDM), and traffic system management (TSM) before considering street widening and network completion.
- M10.2 **Motorized Vehicle Priority Streets.** Support the identification of motorized vehicle streets for arterials with the highest traffic volumes and demonstrated congestion to establish motorized vehicle circulation as paramount to alternative roadway user needs and to encourage investment in congestion relief programs and/or truck safety improvements for the identified routes. Motorized Vehicle Priority Streets are shown in Figure 4-2, Priority Streets.
- M10.3 **Traffic Circulation.** Improve traffic circulation by either making Truman Avenue a one-way street or vacating and closing it north of Hubbard Street in order to remove the redundancy of vehicular circulation space.
- M10.4 **Access Management.** Minimize driveways and consider the addition of medians or designated right-of-way for non-motorized traffic on Arterials to ensure the smooth and safe flow of vehicles, buses, pedestrians, equestrians, and bicycles.

*Motorized Vehicle
Priority Streets*

Street improvements for motorized vehicle priority streets may include peak hour parking restrictions for use of curb lanes, turn lane channelization and traffic signal coordination, and other traffic management techniques to facilitate motorized vehicle flow and discourage cut-through traffic on local neighborhood streets.



San Fernando Road is dedicated as Historic Route 99, as it was the main north-south highway on the west coast of the United States connecting from Mexico to Washington until 1964.



Alleys provide opportunities for the installation of powerlines and other utilities, keeping them away from the fronts of neighborhood streets.

Neighborhood Traffic Control

The quality of life in residential neighborhoods can be adversely impacted by the intrusion of non-residential through traffic. A variety of neighborhood traffic controls exist that can be utilized to regulate, warn and guide movement of pedestrians and vehicular traffic in a safe, efficient and compatible manner. They include stop signs, speed humps, traffic diverters, truck prohibition signs, and right or left turn only lanes. To be effective, they should be clearly understood by motorists and pedestrians. To assure this, traffic control measures need to: (a) convey clear and unambiguous messages, (b) be justified, (c) be enforced, and (d) regulate the traffic for which they are applied and intended.

- M10.5 **Alley Access.** Discourage the vacation and/or closure of existing public alleys in commercial districts and provide for alley access to properties fronting on Arterials
- M10.6 **Emergency Access.** Develop, improve, and maintain hillside streets that are easily accessible to emergency vehicles.
- M10.7 **Coordinated Evacuation Routes.** Establish a network of routes that facilitate orderly evacuation of the community in an emergency, consistent with the Emergency Management Department adopted Evacuation Plan.

Goal M11: Residential neighborhoods that are protected from the intrusion of cut-through traffic, with emphasis on safety and quality of life.

Policies

- M11.1 **Traffic Calming.** Support traffic calming measures and parking management for local and collector streets where demonstrated need exists and with active community involvement, while maintaining pedestrian and bicycle circulation.
- M11.2 **Traffic Mitigations for Development.** Require major developments to mitigate traffic impacts on residential neighborhoods.
- M11.3 **Special Event Coordination.** Encourage coordination of parking ride shuttle services to activity centers and special events such as street fairs and parades.



Traffic calming measures, such as speed bumps, reduce speed and cut-through traffic, making local streets safer for pedestrians and bicyclists.

Goal M12: **Improved air quality and health of residents as a result of decreased single-occupant automobile demand and reduced vehicle miles traveled.**

Policies

- M12.1 **Regional Coordination.** Coordinate with Council of Government and regional transportation planning agencies (such as SCAG and Metro) and adjacent cities to improve shuttle services, encourage ride sharing, bicycle sharing, and other TDM programs within the region.
- M12.2 **Auto Trips.** Create incentives for employers, institutions, and residential neighborhoods to reduce their vehicle trips by encouraging mixed use developments that reduce the number of vehicle miles traveled.
- M12.3 **Alternatives to the Automobile.** Reduce automobile dependency by providing a safe, convenient transit system, pedestrian linkages and a network of safe and accessible bikeways and recreational trails by encouraging alternatives, including reduced emission vehicles, such as electric and neighborhood electric vehicles (NEVs).
- M12.4 **TDM Plans.** Encourage major development to submit a TDM Plan to the City and provide employee incentives for utilizing alternatives to the single-driver automobile (i.e. carpools, van pools, buses, telecommuting, bicycling, and walking, etc.)
- M12.5 **Transportation Management Associations.** Support the formation of agencies and collaboratives such as Transportation Management Associations (TMAs) that facilitate ridesharing in carpools and vanpools.



Share-the-road arrows, also known as sharrows, promote safety and access for bicycles on the street.

Transportation Demand Management

Transportation Demand Management (TDM) is the all-inclusive term given to a variety of measures used to improve the efficiency of the existing transportation system. TDM products and services incentivize alternatives to the single-occupant vehicle and often include the following:

- Formation of a Transportation Management Association
- Subsidizing transit costs for employees or residents
- Flex-time work schedules to reduce congestion at peak times
- Employee parking cashout programs and preferential parking for carpools
- Incentives for walking and bicycling
- Investments in transit infrastructure to increase transit ridership
- Increasing parking prices

Freight Rail Lines and Truck Routes

Freight rail lines and truck routes are identified in the Mobility Plan 2035. Street improvements on truck routes include specialized roadway dimensions to facilitate safe truck movements thereby reducing damage to adjacent property and encouraging trucks to stay on designated routes. Such improvements may include wider traffic lanes and curb return radii, care on overhead signage and additional pavement management considerations. Street on freight rail lines may include dedicated pedestrian and bicyclist crosswalks, signage and pavement markings, and bicyclist dismount area.

Goods Movement

Goods movement is a term used to denote goods or produce transported by ship, plane, train, or truck. Efficient goods movement is crucial to the local and regional economy and an important component of a sustainable, vibrant community. Although trucks are the primary method of transporting goods, freight rail lines represent an essential link in the transportation and delivery of goods throughout the region. Trains, which are more fuel efficient than trucks, can help relieve pressure on overburdened highways and enhance public safety, the environment, and economic development. As the region continues to grow, goods movement and the mitigation of associated impacts to local communities have become increasingly more important, particularly with respect to traffic and train congestion along local corridors, streets and railways.

Most of the truck traffic in Sylmar travels along San Fernando Road, Foothill Boulevard, and Arroyo and Roxford Streets to nearby industrial businesses. A railroad line travels the community along San Fernando Road. Issues of quality of life and public safety are often mentioned by residents and businesses around these areas; as concerns regarding the movement of materials in an efficient, timely and cost effective manner. Controls and limitations exist on truck and train transport to minimize noise and other impacts, avoid damage to infrastructure, and minimize traffic congestion. Enhancing railroad crossings, improving rail connections, and identifying local truck access streets can help decrease traffic delay, reduce rail commuter time, and decrease at-grade crossing accidents. Policies in this section seek to provide for a safe and efficient movement of goods which support commerce and industry, while balancing the needs of other travel modes.



Southward view of the railroad tracks leading through Sylmar to downtown Los Angeles.



The movement of goods is commonly seen on major streets throughout the community.

Goal M13: A community where goods and services can be delivered to its residents and businesses safely and efficiently, while maintaining the community's character and quality of life.

Policies

- M13.1 **Industrial Center Siting.** Site regional distribution centers and other industrial districts proximate to the freeway system, regional truck routes, and rail lines, avoiding adjacency to residential neighborhoods.
- M13.2 **Goods Movement.** Encourage the efficient movement of goods by rail through development of efficient intermodal freight facilities and a shift of a portion of the goods previously moved by trucks onto the rail freight system. Limit truck traffic in residential and commercial areas to designated truck routes.
- M13.3 **Freight Rail Line.** Support continued operation of the regional freight rail system, which offers safe, convenient, and economical transport of commodities.
- M13.4 **Truck and Freight Movement.** Provide appropriately designed and maintained roadways to safely accommodate truck travel and minimize the adverse impacts of freight transport on residential neighborhoods.
- M13.5 **On-site Loading.** Ensure that all commercial and industrial development have adequate off-street accommodations for loading and unloading of commercial vehicles. Minimize potential conflicts between truck loading and unloading and pedestrian, bicycle, and transit access and circulation.



Southeast view from Balboa Bridge overlooking San Fernando Road, the rail-road tracks, and the I-5 and 210 freeway interchange, one the nation's busiest corridor. Photo Credit: Charles Freericks



Freight train entering Sylmar from the north. Photo Credit: Clark Bauman



Los Angeles Mission College converted their surface parking lot into a multiple story parking structure as part of their Master Plan.

Parking Management

Parking Management policies focus on providing sufficient parking for businesses, while protecting adjacent neighborhoods and the environment. It is important to note that parking policies and regulations are closely linked to both the physical and pedestrian character of an area. Well-placed shared parking lots or structures invite customers to park once and then walk to their various destinations. This increased pedestrian activity often spurs even more pedestrian life in commercial districts because other pedestrian-oriented businesses choose to locate nearby.

Parking demand is also affected by the prevalence of nearby transit options. When more people take public transit to a commercial district, the demand for parking in that area declines. For this reason, it can sometimes be appropriate to reduce parking requirements in areas well-served by transit. With this understanding of how parking can impact land use, walkability, and the physical character of an area, this Plan seeks to adequately provide parking for its various uses, while leveraging opportunities for improved parking efficiency that support a more walkable community.

Goal M14: An efficient parking supply that serves economic development and facilitates all modes of transportation.

Policies

- M14.1 **Parking Management Districts.** Support the creation of a parking management district(s) in areas of high demand to facilitate parking within a group of shared facilities.
- M14.2 **Performance-Based Parking Supply.** Utilize performance-based metrics that evaluate existing and projected parking needs in determining parking requirements.
- M14.3 **Conversion of Surface Lots to Structures.** Support the development of City-owned or other surface parking lots into parking structures where appropriate.
- M14.4 **Convenient Parking.** Provide public parking proximate to transit centers, mixed-use boulevards, and public facilities, particularly within the Sylmar/San Fernando Road Metrolink station.

Goal M15: **Parking policies and requirements that support livable neighborhoods, environmental/energy sustainability, and the use of alternative modes of transportation.**

Policies

- M15.1 **Reduced Parking near Transit Centers.** Consider reductions in parking requirements for projects located within 1,500 feet of the Sylmar/San Fernando Metrolink station.
- M15.2 **Park Once Strategy.** Collaborate with the Sylmar Business Improvement Districts to improve parking services along San Fernando Road, Foothill Boulevard, and Glenoaks Boulevard, including shared-parking facilities in appropriate locations to more effectively use the overall parking supply and implement a “park once and walk” strategy for commercial districts.
- M15.3 **Priority Parking for Alternative Fuel Vehicles.** Encourage new commercial and retail developments to provide prioritized parking for shared vehicles, electric vehicles and vehicles using alternative fuels.
- M15.4 **Connections for Electric Vehicles.** Encourage new construction to include vehicle access to properly wired outdoor receptacles to accommodate zero emission vehicles (ZEVs) and/or plug-in electric hybrids (PHEV).



Metro park and ride lots are a convenient way to save money and make commuting to work more enjoyable.



Electric vehicle charging station at the Sylmar/San Fernando Metrolink Station.

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STETSON RANCH EQUESTRIAN PARK

City of Los Angeles Department of Recreation and Parks

PARK CLOSED
SUNSET
TIL
SUNRISE

PARK CLOSED
SUNSET
TIL
SUNRISE



Community Facilities, Infrastructure and Open Space

The ability of the City to provide needed community facilities and infrastructure is crucial to maintaining and improving Sylmar residents' quality of life and to supporting local businesses. Community facilities and infrastructure include police and fire stations, libraries, schools, and parks as well as, water, sewer, and solid waste treatment systems, storm water drainage facilities, the urban forest, street lighting, power transmission lines and distribution stations, and other public utilities. A significant portion of Sylmar consists of open space, which, along with recreation, includes both passive and active open space for recreational facilities, parks, as well as natural resource areas. The urban forest consists of trees and plants that cover an area, on both private and public land. These facilities, infrastructure, and open spaces play an important role in determining the pattern of land uses within the community, where growth should occur, and at what intensities

The purpose of this chapter is to integrate these important public needs into land use decision making when addressing future needs of Sylmar's projected population growth.¹ Infrastructure improvements and new public facilities may be required to support population growth and to replace existing facilities that have deteriorated or become obsolete. This chapter identifies both existing facilities and possible future needs where applicable. This chapter is organized into three general topics:

- Public Facilities – Police, Fire, Libraries, and Public Schools
- Parks, Open Space and the Urban Forest
- Infrastructure – Water, Wastewater, Solid Waste, Stormwater, Energy, and Street Lighting

¹ For further detail about the existing conditions and future demand for most facilities and services, please refer to the Environmental Impact Report (EIR) for the Sylmar Community Plan.

Overview

The Sylmar Community Plan allocates land to accommodate the range of public facilities and open space that the community will need through the life of the plan. This acreage falls within the Public Facilities and Open Space land use classifications. Public facilities, such as police stations, fire stations, libraries, schools and government buildings may be constructed on land designated and zoned for public facilities. In addition, support infrastructure for water, wastewater, stormwater, solid waste and utilities, such as treatment or storage facilities, may also be constructed on land designated for public facilities with certain conditions. Parks and related recreational facilities may be constructed on land within the Open Space and Public Facilities Classification, as well as in all Residential, Commercial and selected Industrial Land Use Classifications.

Several public facilities and capital improvement projects have been constructed within and adjacent to Sylmar since the last community plan update in 1997. Numerous parks and recreational facilities have been maintained and improved, including El Cariso Community Regional Park, Sylmar Recreation Center, and Heritage Park which is on the border of the City of San Fernando. A new elementary/middle school was built on Bledsoe Street and most other public schools have received renovations and repairs. A new community police station was constructed outside of Sylmar that serves the area. The Sylmar Branch Library was modernized and additional instructional and support facilities were constructed at Los Angeles Mission College. In addition, greenbelts and trails were expanded throughout the community, such as the new landscaped bicycle path along the railroad right-of-way, the dedication of the Marge Feinberg Rim of the Valley Trail at the Saddletree Ranch Trailhead, and the development of the trail along the Flood Control Channel leading from Herrick Avenue to Foothill Boulevard. Several improvements to wastewater, stormwater, and other infrastructure were also made, including sewer replacement, installation of new storm drains and catch basins, and modernization of the Sylmar Converter Station.

General Plan Framework

The City's General Plan Framework Element is the citywide plan that establishes how Los Angeles will grow in the future. The Framework Element is a strategy for long-range growth and development, setting a citywide context for the update of Community Plans and citywide elements. The Framework Element, Chapter 9: Infrastructure and Public Services, provides an integrated framework of public facility goals, objectives, policies and implementation measures that incorporate the City's expectations and requirements to allow the effective and efficient provision of public facilities concurrent with need. The Framework Element, Chapter 6: Open Space and Conservation, addresses both publicly- and privately-owned properties that are unimproved and used for preservation of natural resources and outdoor recreation. Addressing public facilities at the Sylmar Community Plan level helps to ensure the Framework's linkage between facility planning and land use by addressing the types of infrastructure required to support the physical development of a specific portion of the City.

Citywide Goals

It is the intent of the Sylmar Community Plan to achieve economy and efficiency in the provision of services and facilities consistent with standards for environmental quality. Cost and distribution are major issues in the provision of services and facilities. It is essential to establish priorities and identify new and different sources of revenue. In addition, public and private development must be coordinated to the maximum extent feasible, in order to avoid expensive duplication and to ensure a balance among needs, services, and cost. The goals and policies in this chapter seek to:

- Achieve economy, efficiency and equitable distribution in the provision of services and facilities consistent with standards for environmental quality.
- Encourage facility-providing departments and agencies to carry out long-range capital facility planning and construction that is compatible with land use planning goals and policies established in the Sylmar Community Plan.
- Fully coordinate public and private development in order to avoid expensive duplication and to ensure a balance among needs, services and costs.
- Require large-scale projects to plan for the siting of necessary public facilities and to provide or fund their fair share of public facility needs created by the development.
- Require discretionary development projects to provide or contribute toward the provision of all public facilities necessary to serve the development.
- Encourage public/private ventures, and other forms of collaboration between government, developers and residents to consider new ideas for providing public facilities and services.

Facilities for Police, Fire, Libraries and Schools

The Framework Element contains citywide goals and policies for the provision of facilities to support municipal operations, including police, fire protection and emergency medical services, libraries and schools. Towards this end, the goals and policies in Chapter 5 seek to:

- Protect the public and provide adequate public safety services, facilities, equipment and personnel to meet existing and future needs.
- Provide library services for current and future community members.
- Ensure adequate school facilities to serve Sylmar's neighborhoods.

Police

Law enforcement services are provided by the City of Los Angeles Police Department (LAPD), which operates 19 stations within four bureaus (Central, South, Valley and West). The LAPD uses a work load computer model (Patrol Plan) to deploy patrol officers to the various geographic areas in the City. This model includes several factors, such as response time, service calls, and traffic conditions.

The Valley Bureau of the LAPD serves the Sylmar Community Plan area and oversees the Valley Traffic Division, as well as seven police stations. One of these seven stations, the Mission Division Community Police Station, located at 11121 North Sepulveda Boulevard in Mission Hills, serves the community of Sylmar, as shown in Figure 5-1, Public Facilities . This facility was built in 2005 and consists of an administration building with a holding tank, a maintenance garage, and car wash. Supplemental law enforcement services are provided by the Los Angeles County Sheriff and the California Highway Patrol when necessary.

Goal CF1: Sufficient police facilities, services, and personnel to protect the community from criminal activity and reduce the incidence of crime.

Policies

- CF1.1 **Level of Service.** Maintain police facilities and services at a level that is adequate to protect the community of Sylmar.
- CF1.2 **Project Review.** Consult with LAPD to consider public safety and crime prevention as part of the review of new development projects (i.e . lighting, visibility, and security) and proposed land use changes to determine needs and services to ensure an appropriate level of service.



LAPD Mission Division Station, serving the Sylmar community since 2005.

Fire and Emergency Services

The Los Angeles Fire Department (LAFD) provides fire prevention, fire protection, and Emergency Medical Services (EMS) to the City of Los Angeles and operates 18 Battalions and 106 neighborhood fire stations located throughout the Department's 470-square mile jurisdiction. The LAFD protects life, property and the environment through fire prevention, firefighting, emergency medical care, technical rescue, hazardous materials mitigation, disaster response, public education and community service.

Sylmar is served by one local fire station and two nearby stations, as shown in Table 5-1, Fire Stations Serving Sylmar and Figure 5-1, Public Facilities. Sylmar has a significant amount of hillside area that is susceptible to fire hazard due to proximity to open spaces, vegetation, and wind conditions. The Los Angeles County Fire Department (LACoFD) and the National Forest Service provide additional services under a Mutual Aid and Assistance Program with the LAFD to provide services to the hillside areas of the community.

LAFD services are based on community needs, as determined by ongoing evaluations. When an evaluation indicates increased response times, the acquisition of additional equipment, personnel, and/or new stations is considered. As development occurs, the LAFD also reviews project-level environmental impact reports and subdivision applications. Development is subject to the standard conditions of the LAFD with regard to building regulations, fire suppression systems, and emergency medical services.



LAFD Sylmar Station No.91, serving the community since the 1960's.

Goal CF2: **Sufficient facilities to provide fire protection and emergency medical services to serve the residents, visitors and businesses in Sylmar.**

Policies

- CF2.1 **Facility Location.** Assist the LAFD to locate fire service facilities in appropriate locations throughout Sylmar to provide adequate fire and emergency services protection.
- CF2.2 **Project Review.** Coordinate with the LAFD during the review of significant development projects and General Plan amendments affecting land use changes to determine the impacts on service infrastructure.
- CF2.3 **Emergency Preparedness Coordination.** Coordinate with the LAFD in the identification of primary access routes for emergency preparedness.

TABLE 5-1
Fire Stations Serving Sylmar

Station	Location
#91	14430 Polk St., Sylmar
#75	15345 San Fernando Mission Blvd., Mission Hills
#98	13035 Van Nuys Blvd., Pacoima

*Source: LAFD Planning Section, April 7, 2009.

*Olive View -
UCLA Medical
Center*

The Los Angeles County Department of Health Services (DHS) provides medical and emergency services to the City of Los Angeles and operates four hospitals: LAC+USC Healthcare Network, Harbor-UCLA Medical Center, ValleyCare Olive View-UCLA Medical Center, and Rancho Los Amigos National Rehabilitation Center. Olive View Hospital, located in the north end of the neighboring community of Sylmar, is one of the primary healthcare delivery systems in the north San Fernando Valley and the closest County hospital serving the Antelope Valley.

Libraries

The Los Angeles Public Library (LAPL) system provides library services through the Central Library in Downtown Los Angeles, eight regional branch libraries, 71 community branches and two mobile book units. The Public Libraries Plan, a component of the Public Facilities Element of the City of Los Angeles General Plan, was adopted by the City Council in 1968 and serves as a general guide for the construction, maintenance and operation of libraries in the City. A new LAPL Branch Facilities Plan was adopted in 2007, which proposed building larger libraries and also established criteria for the size of libraries based on floor area required to serve varying densities of residential population. In general, the recommended sizes are 12,500 square-foot facilities for communities with a population of less than 45,000 and 14,500 square-foot facilities for communities with more than 45,000. In addition, the LAPL plan also recommends that when a community reaches a population of 90,000, an additional branch library should be considered for that area.

Sylmar is served by the Sylmar Branch Library located at 14561 Polk Street, as shown in Figure 5-1, Public Facilities. This library facility was completely renovated in 2003. The library has an area of 12,500 square feet and over 45,000 catalogued books, periodicals, and audio-visual materials. Libraries in the neighboring communities, as well as all branch libraries in the LAPL system augment available library services through their inter-library loan services. Other nearby libraries, including the Library and Learning Resource Center at Los Angeles Mission College and San Fernando's Regional Public Library, provide additional services.



The Los Angeles Public Library's Sylmar Branch is located on the corner of Polk Street and Glenoaks Blvd.

Goal CF3: **Adequate library facilities and services that meet the needs of residents, businesses, employees, and visitors for self-learning, and cultural and academic enrichment.**

Policies

- CF3.1 **Library Services.** Support construction of new libraries and the retention, rehabilitation and expansion of the existing library, as required, to meet the changing needs of the community.
- CF3.2 **Flexibility of Siting.** Encourage new developments to incorporate library facilities in commercial and office buildings, pedestrian-oriented areas, community centers, transit stations, and similarly accessible facilities, particularly in the western portion of Sylmar.
- CF3.3 **Joint Use.** Continue to support joint-use opportunities, especially joint-use with Los Angeles Unified School District and Los Angeles Recreation and Parks, when the City of Los Angeles Library Department and decision-makers review and approve new library sites.
- CF3.4 **Non-Traditional Services.** Expand non-traditional library services, such as book mobiles and other book sharing strategies, where permanent facilities are not available or adequate.

Schools

Public schools in the City of Los Angeles are under the jurisdiction of the Los Angeles Unified School District (LAUSD) which provides public education for over 900,000 students at 557 schools in eight local districts. The LAUSD is subject to the overview of the State of California Legislature and is entirely independent of City of Los Angeles government. Decision making and budgeting are done by an elected governing board, and site and construction standards are established by the State Department of Education (Section 39000 of the Government Code).

Sylmar is located within LAUSD Local District 2, which covers the east San Fernando Valley and includes the communities of Sylmar, San Fernando, Arleta, Pacoima, Sun Valley, Sunland, Van Nuys, Sherman Oaks, and Toluca Lake. The student population in Sylmar is served by 8 elementary schools, a middle school, high school, and continuation schools, as shown in Table 5-2, Public Schools in Sylmar and Figure 5-1, Public Facilities in Sylmar.

The LAUSD's estimate of future enrollment levels and school needs is determined through the evaluation of the capacity of each Local District school to accommodate the projected future population and the analysis of school-by-school enrollment trends. This determination of need is based on several assumptions tied to current LAUSD Board policies and planning guidelines.

In 2000, the LAUSD began a multi-billion building and modernization program. The program includes the construction of several hundred new schools, as well as expansion and modernization projects at existing school facilities to accommodate growth in the student population within and surrounding the City of Los Angeles. Within Sylmar, modernization, renovation, and maintenance projects of varying degrees have taken place at the public schools, including the addition of the Valley Region SPAN K-8 school (Sylmar Leadership Academy) located in the north-central portion of the community.

TABLE 5-2

Public Schools in Sylmar

School	Location
Dyer Elementary	14500 Dyer Street
El Dorado Elementary	12749 El Dorado Avenue
Gridley Street Elementary	1907 8th Street, San Fernando
Harding Elementary	13060 Harding Street
Herrick Elementary	13350 Herrick Avenue
Hubbard Elementary	13325 Hubbard Street
Osceola Elementary	14940 Osceola Street
Sylmar Elementary	13050 Borden Avenue
Sylmar Leadership Academy	14550 Bledsoe Street
Olive Vista Middle	14600 Tyler Street
Sylmar Senior High	13050 Borden Avenue
Evergreen Continuation High	13101 Dronfield Avenue
Los Angeles Mission College	13356 Eldridge Avenue

*Source: LAUSD and Los Angeles Mission College

Goal CF4: **Provision of appropriate locations and adequate public school facilities to serve the needs of the current and future residents in the community.**

Policies

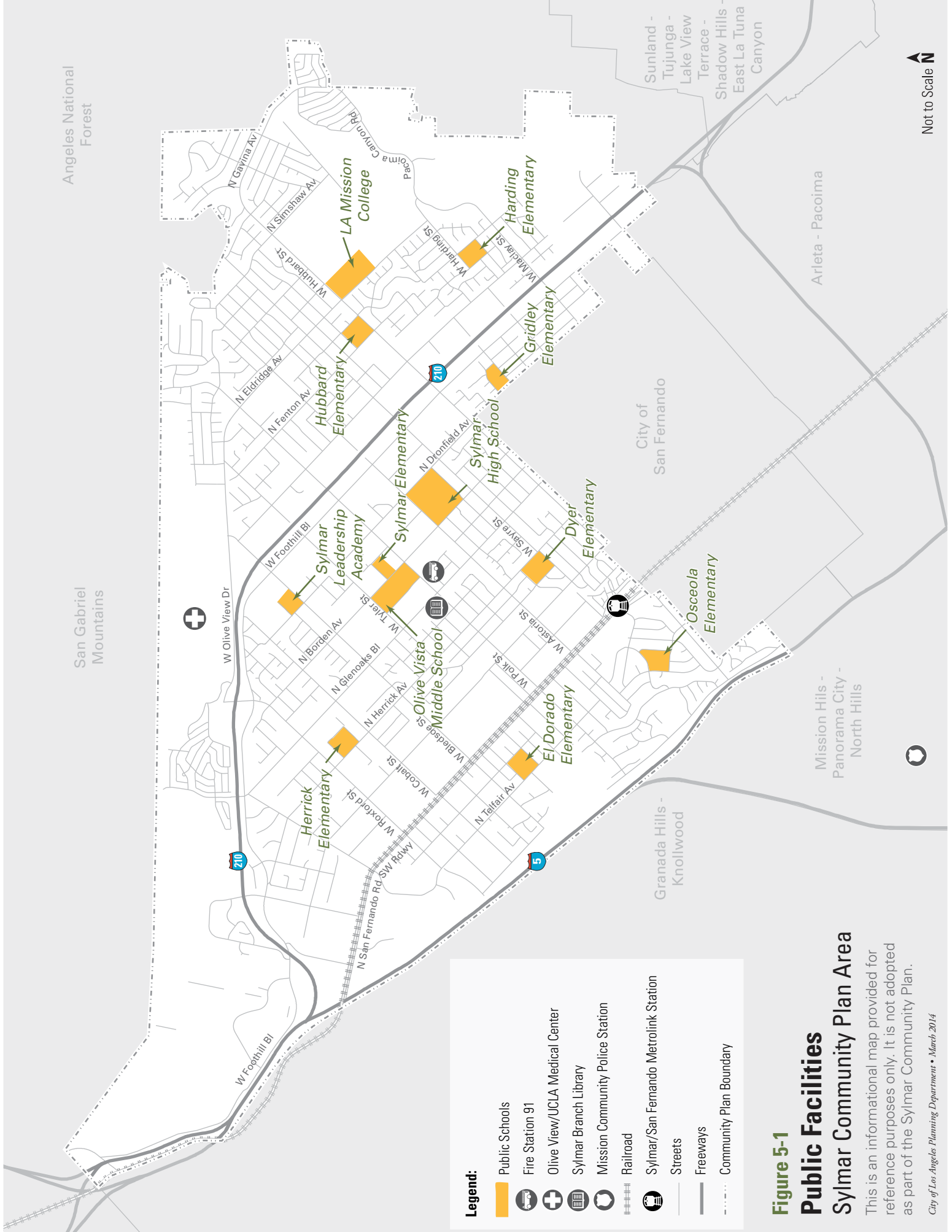
- CF4.1 **Existing Facilities.** Place a high priority on the repair and replacement of any inadequate structural components that threaten the integrity and/or function of instructional buildings. Expansion of existing schools is preferred over the acquisition of new sites, when feasible.
- CF4.2 **Coordination with LAUSD.** Identify future school sites and facilities in a cooperative effort with LAUSD to ensure that they are safely and conveniently located within the Community Plan Area.
- CF4.3 **Facility Location.** Locate new schools in areas with complementary land uses, access to transit, and recreational opportunities. Encourage the siting of schools in locations which can utilize topography and landscaping, as well as building design, to provide noise and air quality buffering, when necessary.
- CF4.4 **Compatible Development.** Encourage compatibility between school locations, site layouts, and architectural designs that contribute to the semi-rural suburban character of the community and that embrace the area’s agricultural heritage.
- CF4.5 **Joint Use.** Coordinate with LAUSD and other agencies to explore creative alternatives that integrate the uses of recreation, local open space, and neighborhood use.

*Los Angeles
Mission College
(LAMC)*

LAMC is a community college located in Sylmar, providing local higher education and vocational training opportunities to residents of the north San Fernando Valley region and surrounding areas. It is part of the Los Angeles Community College District (LACCD). In 2001, California voters approved bond measures to fund the construction, reconstruction, rehabilitation, expansion, and replacement of various LAMC facilities. LAMC adopted a Master Plan for the use of the funds which will be completed in two phases over an eight-year period, through 2015, respectively. Similar to LAUSD, the LACCD is subject to the overview of the State of California.



Olive Vista Middle School provides education to students grades 6-8.



Legend:

- Public Schools
- Fire Station 91
- Olive View/UCLA Medical Center
- Sylmar Branch Library
- Mission Community Police Station
- Railroad
- Sylmar/San Fernando Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

Figure 5-1
Public Facilities
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Parks, Open Space, and the Urban Forest

Parks, open space, and the urban forest are a vital part of a livable, sustainable community. Where housing units may not include yard space and landscaping is scarce, green spaces provide opportunities for passive and active recreation, social and cultural events, and serve as important gathering places in the community. Land designated for open space also allows agricultural and animal keeping uses in some communities, such as Sylmar. The urban forest is part of the community's valuable green infrastructure, and helps reduce the need and expense of building infrastructure to manage air, water, and energy resources.

The Framework, Conservation, Open Space and Service Systems Elements of the General Plan contain citywide goals and policies for the provision of recreational facilities and parks; the provision, management, and conservation of open space for recreation, agricultural, and animal-keeping uses; and management of the urban forest. Towards this end, the goals and policies in this Chapter seek to:

- Protect the City's natural setting from the encroachment of urban development, allowing for the development, use, management, and maintenance of each component of the City's natural resources to contribute to the sustainability of the region.
- Preserve and conserve sufficient open space to serve the recreational and health needs of the City.
- Maximize the use of the City's existing open space network and recreation facilities by enhancing those facilities and providing connections, particularly from targeted growth areas, to the existing regional and community open space system.
- Ensure that the City's open spaces contribute positively to the stability and identity of the communities and neighborhoods in which they are located or through which they pass.
- Preserve and conserve natural resources, scenic areas, and sufficient open space to serve the environmental and safety needs of the City and minimize detrimental impacts.
- Preserve open space for local agricultural and animal-keeping uses within the City, and to protect the semi-rural lifestyle and functions of this community.
- Identify areas for the establishment of new open space opportunities to serve the needs of existing and future residents. These opportunities may include neighborhood parks, urban open spaces, unimproved streets, trails and a citywide linear open space and greenway system that connect the City's regional open spaces, communities, and neighborhoods.

Open Space Designation

The Open Space designation for publicly owned land protects and preserves natural resources and features of the environment such as wildlife refuge and preservation areas, outdoor recreation opportunities, and environmental quality and characteristics. The Open Space designation for privately owned land accomplishes these same goals, as well as preserves land for residential lots zoned for animal-keeping and agricultural uses. Agricultural and animal-keeping zones are also allowed in the Minimum density residential land use designation, and are therefore addressed further in Chapter 3 of this Community Plan. Open Space land may also be used for other types of activities such as public or private management of mineral and other resources and waste.



Legend:

- Open Space
- Parks
- Railroad
- Sylmar/San Fernando Metrolink Station
- Streets
- Freeways
- Community Plan Boundary

**Figure 5-2
Parks and Open Space
Sylmar Community Plan Area**

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Parks

Recreation and parks services in Sylmar are primarily provided by the City of Los Angeles Department of Recreation and Parks (RAP) and the Los Angeles County Department of Parks and Recreation (LACoDPR). There are four types of parks:

- **Mini/Pocket Parks** are designed to provide small spaces for limited types of recreational activities to an immediate adjacent neighborhood, and are typically established as part of a new residential or commercial development.
- **Neighborhood Parks** provide space and facilities for outdoor and indoor recreation activities to all residents in the residential area within a ½ mile radius of the park.
- **Community Parks** provide a broader range of services and satisfy the needs of the nearby community as well as other service areas within a 2 mile radius of the park.
- **Regional Parks** are intended to serve the entire region and typically provide specialized recreation facilities such as lakes, golf courses, campgrounds, wilderness and museums.



Sylmar Recreation Center offers afterschool recreation and activities.

Recreation and Parks

Recreational opportunities improve quality of life and advance public health and welfare. Parks and recreation facilities can be publicly owned as well as privately-owned, provide opportunities for exercise, nature exploration, and other recreational activities. Examples of privately owned open space and recreation include land owned by land trust and conservancy organizations, wildlife rehabilitation centers, plazas and open space provided by private developers, and golf courses. Parks and related recreational facilities may be constructed on land within the Open Space and Public Facilities land use classifications, as well as in all Residential and Commercial and some select Industrial zones. Open space is typically publicly- or privately- owned land that remains undeveloped for the purpose of protecting and preserving natural resources and features of the environment, as well as for recreation. More information about open space, its purpose, and the uses allowed in the Open Space designation, can be found in Chapter 3, Land Use.

Planning and implementation of parks and recreation assets and amenities is based on a standard of population density to ensure that resources are allocated with the goal of providing the same level of facilities and services to all residents. The Public Recreation Plan of the City of Los Angeles provides the official guide for considering minimum needs of neighborhoods and communities for recreational sites.

As shown in Figure 5-2 and Table 5-3, Parks and Open Space, Sylmar has a total of seven parks, including three regional parks, two community parks, and two neighborhoods parks with over 567 acres that play an important role in the physical, social, and mental health of the residents and visitors of the community.

**TABLE 5-3
Parks and Open Space in Sylmar**

Park/Open Space	Location
Carey Ranch	15000 Crestknoll Drive
El Cariso Community Regional Park	13100 Hubbard Street
El Cariso Golf Course	13100 Eldridge Avenue
Heritage Park	2025 4 th Street, San Fernando City
Stetson Ranch Equestrian Park	13877 Glenoaks Boulevard
Sylmar Park Recreation Center	13109 Borden Avenue
Veterans Memorial Community Regional Park	13000 Sayre Street
Wilson Canyon Park	14450 Olive View Drive

*Source: LADCP

Regional parkland includes El Cariso Regional Community Park and Golf Course, Veterans Memorial Community Regional Park, and Wilson Canyon Park. With the exception of Wilson Canyon Park, both parks are owned and operated by the Los Angeles County Department of Parks and Recreation (LACoDRP). Wilson Canyon Park is owned and operated by the Santa Monica Mountains Conservancy.

- **El Cariso Community Regional Park and Golf Course** are located in the eastern portion of the community, adjacent to Los Angeles Mission College, in the foothills of the San Gabriel Mountains. El Cariso Community Regional Park provides amenities such as picnic areas, play grounds, basketball courts, a baseball diamond, tennis courts, walking paths, a swimming pool, and a fitness zone. Community classes, after school programs, summer and winter camp programs are also offered through the LACoDRP. El Cariso Golf Course provides banquet facilities, a cocktail lounge, a coffee shop, and a club house in addition to the 18-hole driving range and three practice putting greens and lakes provided on-site.
- **Veterans Memorial Community Regional Park** offers rolling hills, picnic areas, children play area, camping grounds, a frisbee golf course, children’s play area and a variety of hiking and riding trails leading north up into the San Gabriel Mountains and Angeles National Forest. This parkland is situated against the foothills in such a way that only a portion of the parkland is actually located within Sylmar’s boundaries.
- **Wilson Canyon Park** is situated in the foothills above Olive View Medical Center, offering visitors a secluded wilderness setting, a network of easy to moderate trails, and a rich variety of flora and fauna. Similar to Veteran’s Memorial Community Regional Park, only a small portion of this parkland is included within the boundaries of Sylmar while the majority lie within unincorporated Los Angeles County land.



Shaded picnic area at El Cariso Regional Park

Community parkland includes Sylmar Recreation Center and Stetson Ranch Equestrian Park, both of which are owned and operated by the RAP.

- **Sylmar Recreation Center** serves is centrally located within the community and is within close proximity to Olive Vista Middle School and Sylmar High School. Amenities include an auditorium, an indoor gymnasium, a community room, basketball courts, tennis courts, a children’s play area, a baseball diamond, a soccer field, picnic tables, a swimming pool, and community classes and after school programs are also offered through park services.
- **Stetson Ranch Equestrian Park** is located at the base of the Angeles National Forest and offers hiking, biking, and equestrian trails, as well as equestrian staging areas and a riding arena. The Saddletree Ranch Trailhead is accessible from within Stetson Ranch Park, providing multi-use access to the Marge Feinberg Rim of the Valley Trail and a linkage to Wilson Canyon Park.

Neighborhood parkland includes Carey Ranch Park and Heritage Park. Carey Ranch Park is owned and operated by the RAP and Heritage Park by the City of San Fernando.

- **Carey Ranch Park** is a neighborhood park located in the southwestern portion of the CPA, and provides local residents with open space for sport activities, a children’s play area, and picnic tables. The park is located on top of a hill above the neighborhood, and provides a peaceful view overlooking Sylmar and other communities of the San Fernando Valley.
- **Heritage Park** is a neighborhood park partially located in Sylmar which provides local residents with passive open space and a conservation garden with native and riparian plantings and dry stream bed system which retains storm water.



Sylmar Flight Park used by the hanggliders as a safe landing zone.

There are also several private parkland and recreational facilities that are available to the community for recreational enjoyment, including: a mini/pocket park on the corner of Telfair Avenue and Cobalt Street, the Cascade Golf Course, the Sylmar Flight Park Landing Zone, Sylmar Independent Baseball League, and the Wildlife Learning Center.

In addition, Sylmar is served by a Trails System that is intended for walking, hiking, and equestrian activity leading up to the foothills and connecting with the Rim of the Valley Trails System. Some portions of the trails have been improved with fencing and resurfacing, while many exist as unimproved sidewalks. Goals, policies, programs, and design guidelines for the development of trails are further addressed in the Land Use and Mobility chapters of this Community Plan, as well as the Open Space, Conservation, and Mobility Elements of the General Plan.

Opportunities to acquire undeveloped land for park purposes are decreasing. When surplus public property is offered for private sale, it reduces land that could be used for public parks. Efforts must be made to determine if such property could be used to make up a deficiency in much needed park acreage. Priorities for new parks and open space have been identified by the community, including:

- Park space should be distributed throughout Sylmar, with attention to those areas most deficient in open space such as the areas south of the Foothill Freeway (I-210). Opportunities for smaller mini/pocket parks are more likely in this area.
- Non-traditional open space resources should be considered, such as vacated railroad lines, drainage channels, planned transit and utility-right-of-ways, public plazas, and other public spaces as potential opportunities for active and passive recreational uses.



Stetson Ranch's riding rink is a popular attraction.

- New park development should focus on creating more walking, biking, and nature trails, and athletic fields and play spaces for children and teens, such as soccer fields and skate parks. There is also an overall need for more pocket parks, community garden space, and an outdoor amphitheater for program events.
- Public facilities and recreational areas are in need of some enhancements. There is a need for more youth, family and culturally-oriented programs, better playground equipment, security lighting and on-site security in parks, improvements to existing sporting fields and aquatic areas, and public transportation to and from parks.

Goal CF5: **A variety of well-maintained parks and recreation facilities and services that meet the existing and future recreational needs of the community.**

Policies

- CF5.1 **Parkland Preservation.** Protect parklands from uses that would result in a loss of acreage for recreational purposes.
- CF5.2 **Site Enhancements.** Enhance and improve all parks and recreation areas by providing amenities where appropriate, such as pedestrian paths, and bike and equestrian trails.
- CF5.3 **Surplus Property.** Coordinate with the RAP and other applicable City departments, such as the Department of General Services and Department of Transportation, to review and evaluate surplus property as potential sites for parks and recreational activities.



Walking paths and mature trees are common amenities found at all parks within the community.

- CF5.4 **Vacant Land.** Encourage continuous efforts by public agencies to acquire vacant parcels for publicly owned open space and parks.
- CF5.5 **Public Right-of-Ways.** Support the creation of new parks and park expansions within public right-of-ways, such as flood control channels, utility easements, debris basins, and other unused and underutilized public properties. Hiking, bicycle, and equestrian trails in Sylmar should connect these facilities with the Angeles National Forest, Hansen Dam Recreation Area, El Cariso Regional Park, and the Pacoima Wash.
- CF5.6 **New Development.** Encourage and allow opportunities for new development to provide pocket parks, small plazas, community gardens, commercial spaces, and other gathering places that are available to help meet recreational demands.
- CF5.7 **Location.** Encourage neighborhood parks and recreational centers near concentrations of residential areas and include safe pedestrian walkways and bicycle paths that encourage non-motorized use.
- CF5.8 **Design.** Encourage the development of new park facilities to contribute to the semi-rural suburban character of the community by utilizing architectural features and building materials that embrace the area’s agricultural and equestrian heritage.
- CF5.9 **Joint-Use.** Support the establishment of joint-use agreements with other public and private entities to increase recreational opportunities in Sylmar, including shared use of land owned by public agencies and private property owners.
- CF5.10 **Public Transit.** Coordinate with the appropriate departments and agencies to create public transit that can connect neighborhoods to regional parks.
- CF5.11 **Park Safety.** Promote the design, construction, maintenance, and management of public parks to ensure that parks are adequately monitored, maintained, and illuminated at night, especially for families with children and senior citizens who use the parks.

Open Space

Open Space, broadly defined as land which is essentially free of structures and buildings and/or is natural in character, encompasses both publicly and privately owned properties that are unimproved. Maintaining land as open space is a way to preserve natural resources, wildlife corridors, outdoor recreation, manage mineral and other resources, protect life and property due to natural hazards, and to connect neighborhoods and people. Open Space is also a land use in the City's land use classification system.

Although the majority of Sylmar is developed suburban land, significant areas remain as undeveloped or natural open spaces. Most of the open space is located around the northern and eastern borders of the community plan boundary, adjacent to the Angeles National Forest and San Gabriel Mountains, as shown in Figure 5-2, Parks and Open Space. Larger open space parcels that remain are primarily owned by the City of Los Angeles, Los Angeles County Public Works, Metropolitan Water District, Santa Monica Mountains Conservancy, and the United States Army Corps of Engineers.

The largest area of open space within Sylmar is the Pacoima Wash, which is located in the eastern portion of the community and is jointly owned and operated by the US Army Corps of Engineers, LA County Flood Control, and the Metropolitan Water District. This area includes undeveloped hillsides in the northern segment where the Pacoima Dam is located, and the un-channelized river bed leading to the Lopez Debris Basin (which traps sediment and regulates water at a constant volume as it passes into a concrete lined channel before entering the City of San Fernando and eventually joining with the Tujunga Wash). The Pacoima Wash, together with the Tujunga Wash, creates the largest sub-watershed of the upper Los Angeles River. This area is commonly referred to as the "Upper Watershed" and is relatively undisturbed



The Pacoima Wash is a valuable community resource, providing natural means for groundwater infiltration, watershed management, and habitat conservation.

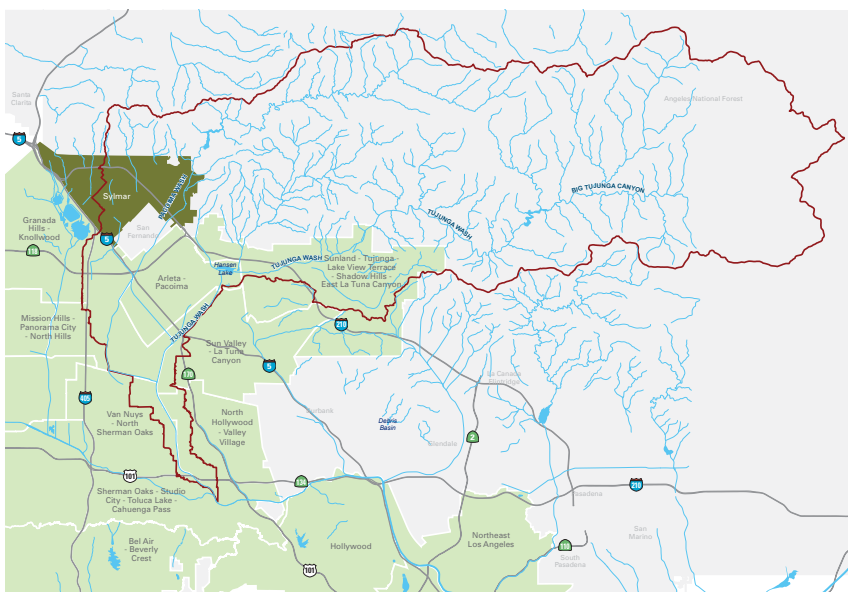
open space where the wash is un-channelized and in its natural state, maintaining a stream flow which allows for the erosion and deposition of soils, rocks, and alluvium, changing its appearance and flow over time. In addition, these changing conditions combined with the un-channelized characteristic of the Wash allow for water to seep into the porous streambed, recharging the underlying groundwater table.

The un-channelized portion of the Wash is unique in that it provides an important source of potable water for the region that the channelized portions of the lower segment of the Wash does not provide. This upper segment of the watershed also acts as a transitional buffer between the rugged slopes of the San Gabriel Mountains and the suburbanized residential neighborhoods located within the City. This area allows for the creation of small micro habitats that support wildlife adapted to these conditions. It also provides a gateway for both wildlife movement throughout the watershed and access to scenic and recreational resources for the residential population. Recreational and educational facilities within close proximity or adjacent to the Wash include LA Mission College, El Cariso Park and Golf Course, SIBL baseball fields, the Sylmar Hang Gliding Association’s Flight Park, and the Rim of the Valley Trail network.

Other significant areas of open space in the community include Stetson Ranch Equestrian Park and Wilson Canyon Park, which offers a secluded wilderness setting, networks of easy-to-moderate trails, and a rich variety of plant communities, owned and operated by the Los Angeles Department of Recreation and Parks and the Santa Monica Mountains Conservancy, respectively. These open space areas also provide wildlife access to the corridors leading up to the San Gabriel Mountains and the Angeles National Forest and are valuable recreational assets for community members and visitors who enjoy hiking, equestrian riding, mountain biking, bird watching, and picnicking.

Tujunga/Pacoima Watershed

The community of Sylmar is located within the Tujunga/Pacoima Watershed, the largest subwatershed of the Los Angeles River Watershed. Historically, groundwater provided the water supply to the watershed but since their channelization the groundwater levels have dropped. The Tujunga/Pacoima Watershed is comprised of three major drainage systems (Pacoima Wash, Big Tujunga Wash, and Little Tujunga Wash), two groundwater basins (San Fernando and Sylmar), and four reservoirs (Big Tujunga, Hansen, Pacoima, and Lopez). The Pacoima Wash and Sylmar Groundwater Basin are located within Sylmar and connect to the Pacoima Dam just north of Sylmar.



Tujunga-Pacoima Watershed



Trail along the flood channel adjacent to Olive View/UCLA Medical Center, provides.

As opportunities for conventional open space resources diminish, it is important to identify areas that have not traditionally been considered as open space resources and work in collaboration to enhance and transform these areas for public enjoyment. These opportunity areas include railroad lines, drainage channels, planned transit, utility-right-of-ways, pedestrian-oriented streets, privately developed mini/pocket parks, and trails. There is also a need to protect existing ecological and cultural resources in Sylmar through passive park and open space uses.

Opportunities exist throughout Sylmar, especially within the Pacoima Wash, to develop a watershed-wide network of pedestrian, equestrian, and bicycle facilities, connecting existing and planned recreational facilities and communities, as shown in Figure 5-3, Pacoima Wash Greenway Network. The limited number of intersections within the Pacoima Wash provide a unique opportunity for distance and integral recreational activities such as biking and distance running. The character of the Pacoima Wash also allows for enhancements that could transform the waterway into a scenic corridor with habitat conditions for many types of native plants and animals to thrive. This area is an important focal point for the community and provides direct access to the San Gabriel Mountains, Angeles National Forest, and the Rim of the Valley Trails Corridor.

There are also opportunities to expand and enhance other areas of underutilized open space in Sylmar through coordination and planning with the different agencies that operate these respective lands. Areas owned by the Department of Water and Power and the Los Angeles County Flood Control have the potential to provide recreational opportunities, such as trails and paths for a variety of users, while improving the water quality, increasing flood protection, and capturing and infiltrating storm water for groundwater recharge.

Goal CF6: A community with sufficient open space in balance with new development to serve the recreational, environmental, health and safety needs of the area and to protect environmental and aesthetic resources.

- CF6.1 **Conservation.** Preserve passive and visual open space that provides wildlife habitat and corridors, wetlands, watersheds, groundwater recharge areas, and other natural resource areas.
- CF6.2 **Protection.** Protect significant open space resources and environmentally sensitive areas from environmental hazards and incompatible land uses.
- CF6.3 **Grading.** Minimize the grading of natural terrain to permit development in hillside areas and the foothills correspond with densities designated by this Community Plan, the geological stability of the area, and compatibility with adjoining land uses.
- CF6.4 **Stream Alterations.** Minimize the alteration of natural drainage patterns, canyons, and water courses, except where improvements are necessary to protect life and property.
- CF6.5 **Development Restrictions.** Restrict development on areas of known geologic hazard, unstable soil conditions or landslides.

- CF6.6 **Ecologically Sensitive Areas.** Coordinate with County of Los Angeles in the identification of significant ecological areas featuring ecological or scenic resources that should be preserved and protected within State reserves, preserves, parks, or natural wildlife refuges.
- CF6.7 **Open Space Integration.** Integrate the use of open space with public facilities adjacent to reservoirs, land reclamation sites, spreading grounds, power line right-of-ways and flood control channels.
- CF6.8 **Trail Linkages.** Continue to expand and maintain trail linkages which reinforce the viability of equine uses and accessibility to open spaces by designing development and infrastructure improvement projects that abut or connect with a trail to develop and/or improve the Trail Network. Refer to Chapters 3 and 4 for additional trail-related policies and design guidelines.
- CF6.9 **Greenways.** Establish, where feasible, multi-use greenways along waterways, rail lines, and utility corridors to provide additional open space for passive or active recreation and to connect adjoining neighborhoods to one another and to regional open space resources.
- CF6.10 **Access and Connections.** Improve connectivity and access to the Rim of the Valley Trails corridor and other adjacent open space resources using such tools as easements and trail and greenway linkages.

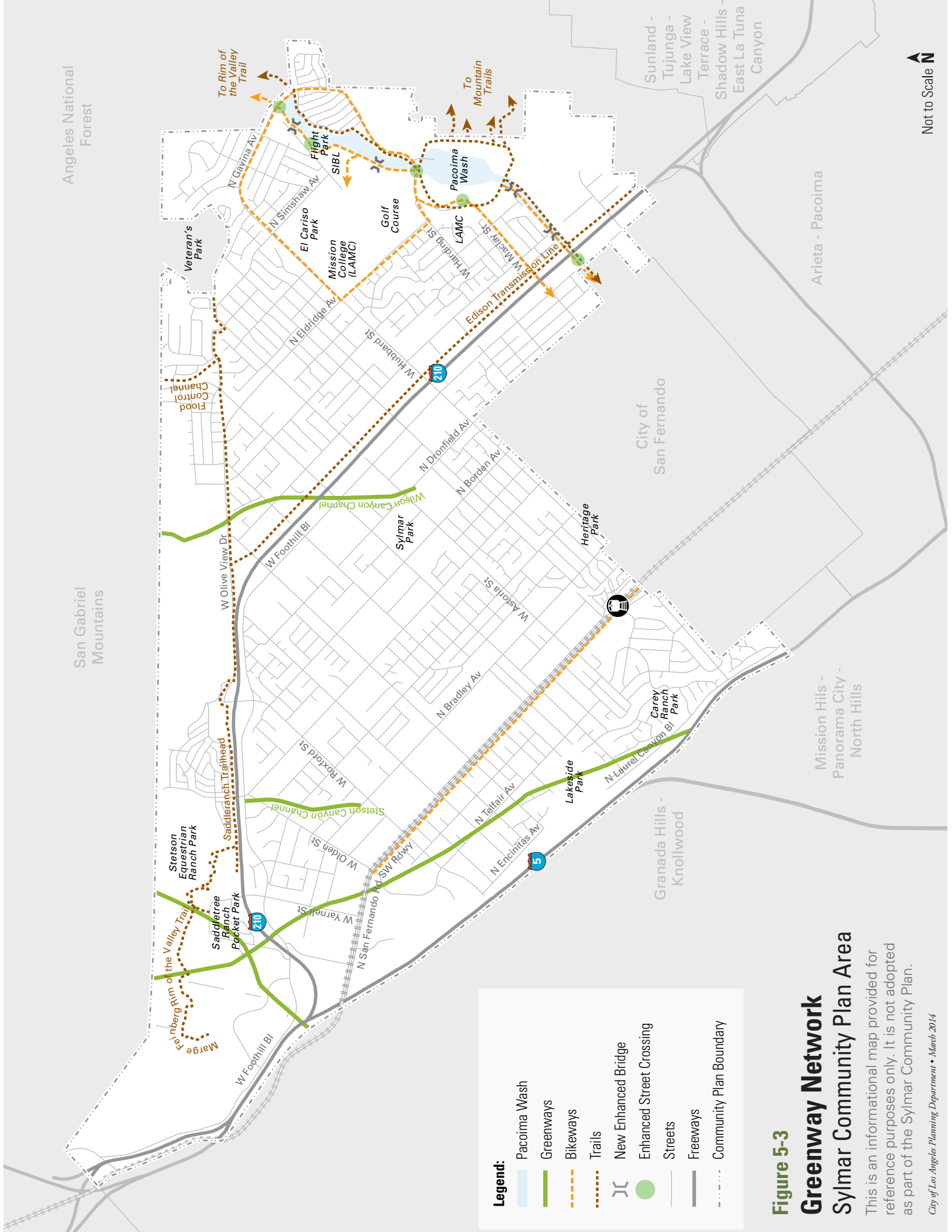
Trails as Valuable Open Space

Trails offer recreational opportunities to explore the City's open spaces, parks, and other Trail Networks on foot, horseback, or bicycle. The Rim of the Valley Trail Network is a system of existing and proposed trails that connect various open spaces.

The Trail Network map, as shown in Chapter 4, serves as a guide for trail development and includes both the Rim of the Valley Trail Network as well as other existing and proposed trails. While many trails in the City of Los Angeles are on public land in parks and on public streets, there are also trails within easements on privately-owned land and private streets, as well as informal trails that community members have traditionally used and which traverse undeveloped privately-owned land. Chapter 3, 4, and 5 contain goals, policies, and design guidelines supporting trail development, improvement and connectivity. In addition, the Equinekeeping "K" District Ordinance contains conditions, in addition to the Los Angeles Municipal Code (LAMC) requirements, which support equinekeeping on residential lots and the Trail Network.



Scenic trail and open space connections.



- Legend:**
- Pacoima Wash
 - Greenways
 - Bikeways
 - Trails
 - New Enhanced Bridge
 - Enhanced Street Crossing
 - Streets
 - Freeways
 - Community Plan Boundary

Figure 5-3
Greenway Network
Sylmar Community Plan Area

This is an informational map provided for reference purposes only. It is not adopted as part of the Sylmar Community Plan.

Goal CF7: **A beautiful, functional Pacoima Wash and greenways network with vibrant wildlife habitats, enhanced open spaces for passive and recreational uses, and connects adjoining neighborhoods and communities to one another and to regional open space resources.**

- CF7.1 **Wildlife Habitats.** Support enhancements that transform the Pacoima Wash into a natural scenic corridor with habitat for many types of native plants and animals, while maintaining and/or improving flood control protection and water quality.
- CF7.2 **Pacoima Wash Greenway Network.** Link existing and planned educational and recreational facilities in and around the Pacoima Wash, including El Cariso Regional Park, Los Angeles Mission College, the hang gliders park and baseball fields, and other nearby open space and recreational areas, which increase opportunities for active and passive recreation and create a focal point that promotes healthy lifestyles. Refer to Figure 5-3, Greenway Network.
- CF7.3 **Connectivity and Access to Open Space.** Maintain and improve connectivity and access to the Pacoima Wash, San Gabriel Mountains, Angeles National Forest, Rim of the Valley Trails corridor, and other open spaces and adjacent neighborhoods.
- CF7.4 **Multi-purpose Greenways.** Utilize tools such as easements and greenway linkages, including walking, hiking, trails, and bicycles paths that are maintained accessible to the public, which provide a safe non-motorized transportation path for all users and expand park and open space opportunities.
- CF7.5 **Greenway Bridges.** Support enhancements to existing bridges in order to accommodate a variety of users along the Pacoima Wash. The design of bridges should complement the surrounding area, be built with materials in keeping with the aesthetics of the area, and each bridge should be as wide as the trail it serves.
- CF7.6 **Crossings.** Enhanced trail crossings are recommended along Harding Street, across Maclay Street between Los Angeles Mission College, and Gavina Street to ensure safety and connectivity of the various trails.
- CF7.7 **Development.** Development which occurs in proximity to desirable open space areas should include roads and trails adequate to serve both that development and the immediate adjacent recreation and open space areas.
- CF7.8 **Right-of-Ways.** Improve available right-of-ways with landscaping, benches, walkways, trails, and bikeways for low-intensity recreational uses.
- CF7.9 **Wayfinding.** Wayfinding and interpretive elements should be installed at key intersections in the areas surrounding the Pacoima Wash to direct people to the Wash and important points of interest and to provide information on the ecology, wildlife, and history of the Wash and surrounding area.

Urban Forest

The urban forest is comprised of all the privately and publicly maintained trees and naturally occurring vegetation (i.e. hillside chaparral, riparian areas) growing in an urban area. Street trees are a significant and highly visual portion of the urban forest and recognized as a vital infrastructure system essential to the quality of life in the urban environment. When properly planted and managed, the urban forest provides ecological, social, aesthetic, and economic benefits by improving environmental quality, reducing storm water runoff and soil erosion, and conserving energy. The urban forest also contributes to the preservation of the ecosystem by preserving stretches of urban forest along common wildlife migration corridors, such as floodplains, making the survival of various species in urban settings more likely.

The Department of Public Works Bureau of Street Services, Urban Forestry Division (UFD) is responsible for the care and preservation of trees and landscaped areas in the public right-of-ways, such as street trees and landscaped traffic medians, as well as the creation and development of street tree policies and guidelines. The UFD estimates that there are nearly 700,000 street trees growing along miles of public roads throughout the City with over 1,000 different species, varieties, and cultivators. Trees growing within City parks are maintained separately by the Forestry Division of the Recreation and Parks (RAP). RAP estimates that there are at least one million trees growing naturally and in the City's developed urban parks.

Sylmar contains an abundance of trees, many of which are remnants of the olive tree and citrus orchards that were planted at the turn of the 20th Century. In addition to the heritage olive trees still remaining, there are many other types of trees on both public and private land that add to the vast landscape and network of the "Sea of Trees" that exists in Sylmar.



Sylmar's healthy tree population is clearly visible from higher elevation points within the community.

Preservation of the urban forest ecosystem is critical to achieving the goals and policies set forth in this community plan, including the quality of life for the residents and visitors of Sylmar. As new development occurs in the community and the population continues to grow, there is great opportunity to increase and expand Sylmar’s urban forest on both private and public land.

The UFD and the RAP Forestry Division separately encourage community involvement and public-private partnerships to plant and maintain the urban forest along public right-of-ways and in public parks. Through proper planning, conflicts between street trees and other vital infrastructure could be minimized, as well as the costs associated with the preservation of a healthy and safe urban forest.

Goal CF8: The preservation of a healthy and safe tree population in all neighborhoods to maximize the benefits gained from the urban forest, such as air quality improvement and aesthetic enhancement.

- CF8.1 **Urban Forest.** Encourage the preservation of the existing tree population and include new trees in an effort to achieve optimum canopy cover to reduce and mitigate the heat island effect. Include on-site trees in new development projects, whenever possible.
- CF8.2 **Tree Protection.** Encourage and promote the retention of trees, particularly olive and orange trees, where practical and appropriate, through education, outreach and incentives offered by the Bureau of Street Services.
- CF8.3 **Tree Selection.** Support policies of the Bureau of Street Services to reduce conflicts with existing infrastructure through proper tree selection and through the recognition of street trees as a vital component of the City’s infrastructure.



Newly planted street trees and landscaping help to reduce the urban heat island effect, even in industrial areas.

- CF8.4 **Native Plants.** Encourage the use of plant communities native to Los Angeles which achieve native biodiversity and enhance existing wildlife habitats.
- CF8.5 **Shade Streets.** Facilitate the planting and maintenance of street trees, which provide shade and give scale to residential and commercial streets in all neighborhoods in Sylmar.
- CF8.6 **Sustainable Design.** Develop design standards that promote the sustainable development in public and private open space and street right-of-ways.
- CF8.7 **Partnerships.** Encourage community and private partnerships in urban forestry issues, minimizing maintenance costs. Collaborate with other City departments, neighborhood associations, business improvement districts and private developers to promote trees in parkways, landscaped medians, community gateways, and throughout Sylmar.



Street trees provide shade and make for a more enjoyable streetscape environment.

Infrastructure

This section addresses physical infrastructure that is provided by the City of Los Angeles Department of Public Works and the Los Angeles Department of Water and Power (LADWP). These systems include water, wastewater, stormwater, solid waste management, energy (power) and street lighting. The infrastructure for these systems is of a citywide nature rather than local to a particular Community Plan Area. For example, a wastewater treatment facility may be located in one Community Plan Area, but provide service to several Community Plan Areas. Additionally, this infrastructure may be underground, or located in areas that are not visible to community residents.

The policies included in this section for water, wastewater, solid waste, stormwater, energy, and street lighting are specific to Sylmar. The Framework Element, described earlier in this chapter, contains citywide goals and policies for the provision, management, and conservation of water, wastewater, solid waste, stormwater, energy and street lighting in this section. Towards this end, the goals and policies in Chapter 5 seek to:

- Provide for the existing and future infrastructure needs of the City that support the basic public services necessary to maintain and improve its quality of life.
- Encourage watershed-based planning and projects in order to reduce stormwater runoff, optimize local water resources and reduce dependence of imported water, improve surface water and groundwater quality, and restore hydrologic function to the watershed while maintaining public safety.
- Encourage public/private ventures and other forms of collaboration between governments, developers, and residents to consider new ideas for providing infrastructure and services.

Water

The Los Angeles Department of Water and Power (LADWP) is responsible for ensuring that water demand in the City is met and that state and federal water quality standards are achieved. The LADWP is the nation's largest municipal utility, and its service area is slightly larger than the boundary of the City of Los Angeles. Under the provisions of the Los Angeles City Charter, the LADWP has control of the water distribution system within the boundaries of Los Angeles. Water supply boundaries are not divided by community plan area, but rather determined by pressure zones that are dictated by ground elevation.

The California Urban Water Management Planning Act requires water supplying agencies to prepare and adopt water management plans every five years to identify short-term and long-term water resources management measures for meeting growing water demands. The LADWP 2010 Urban Water Management Plan (UWMP) is designed to meet the current requirements of the Act and also serves as the City's master plan for water supply and resource management. This plan provides an assessment of current water system conditions (source of supply, treatment, transmission, storage and distribution) for capacity to meet projected demands to 2035. A primary objective of this plan is to pursue cost-effective water conservation and recycling projects to increase supply reliability and offset increases in water demand due to growth and other factors.

The City obtains its water supply primarily from four major sources: Los Angeles Aqueduct, groundwater wells, purchases from the Metropolitan Water District (MWD), and wastewater that is treated and recycled for reuse in industrial facilities and for irrigation purposes. The first three sources have historically delivered an adequate and reliable supply of water to serve the City's needs. However, expansion of recycled water projects and water conservation and groundwater infiltration measures are planned to fill a larger role in the City's water supply portfolio.



Los Angeles Aqueduct Cascades

Water supply infrastructure consists of water storage facilities, transmission and distribution pipelines, booster pumping stations, pressure reducing stations, and other related facilities. The City's water system contains 110 tanks and reservoirs and 7,200 miles of water mains that distribute water. Sylmar has 5 water storage facilities located throughout the community.

Portions of Los Angeles are served by MWD, including Sylmar. The MWD is committed to continue to plan for emergencies and natural disasters throughout the region. The agency has approximately 1.7 million acre-feet in surface and groundwater storage accounts, including Diamond Valley Lake near Hemet, and 600,000 acre-feet of storage reserved for emergencies. This reserve of water supplies buffers the severity of a potential shortage, allows for a less severe water shortage allocation if required, and keeps the region prepared for major earthquakes or other natural disasters. The community of Sylmar is currently supplied primarily through MWD water sources.

The LADWP is also increasing the safety and water quality of the water delivered to customers by reducing daily dependence on large in-city open reservoirs that have historically been the City's water source. Due to a culmination of regulations dealing with runoff into open reservoirs, increased disinfection standards, and byproducts created during disinfection, no open reservoirs will be allowed to remain in service in the City's water distributions system. Additional trunk lines are necessary to ensure that sufficient quantities of water can be moved from one area of the city to another when needed.

A number of factors, including the projected increase in water consumption overall, may require the upgrading or expansion of existing local distribution systems within Sylmar. Many structural components and facilities are 50 to 90 years old and are no longer meeting the performance and quality standards expected. Citywide, this extensive water system will require significant capital improvements due to aging infrastructure, schedule replacements, and anticipated changes in state and federal water quality regulations.

Groundwater Basins

The capture, storage, and infiltration of snowmelt and stormwater in the City's groundwater basins are critical to local water supply and have the potential to increase the amount and reliability of local supplies and reduce reliance on imported sources that would save energy and reduce greenhouse gas emissions. The Tujunga/Pacoima Watershed, with concentrated rainfalls, steep slopes, pervious soil, and a capacious underground reservoir, offers the City the strongest opportunity to secure a sustainable local water supply.

The LADWP has developed several water recycling projects in partnership with local, State, and Federal agencies in order to help meet the City's future water demand. The East Valley Water Recycling Project, housed at the Donald C. Tillman Water Reclamation Plant located in the Sepulveda Basin, is one of four recycling projects operated by Los Angeles Bureau of Sanitation (LABOS) and LADWP. This water is delivered to industrial and commercial businesses for non-drinking and irrigation purposes throughout the San Fernando Valley, and is also used to recharge the underground reservoir in the northeast San Fernando Valley. In addition, the LADWP UWMP has identified other potential water sources, including water transfers, gray water, and beneficial uses of stormwater and urban runoff.

Opportunities exist throughout Sylmar, in particular within the Pacoima Wash and surrounding area, to replenish the aquifer by restoring the natural hydrologic functions of the watershed. The Pacoima Watershed was a major contributor of groundwater supply. The community sits atop the Sylmar Groundwater Basin, a natural underground reservoir that has become depleted over the years as most of the valley floor became impervious. The Sylmar groundwater basin has the potential to provide much more of the City's drinking water needs.

Goal CF9: Provision of a high-quality, reliable supply of potable water to existing and future residents and businesses of the Sylmar community.

- CF9.1 **Local Water Resources Optimization.** Meet increases in the demand for water through conservation, the use of recycled water for irrigation, non-potable, and non-residential uses, and by recharging the local groundwater aquifers where permitted, to reduce dependence on imported water.
- CF9.2 **Water Conservation.** Require water conservation measures/devices that limit water usage for all new municipal and private projects, and major alterations to existing municipal and private facilities.
- CF9.3 **Alternative Sources.** Support the development of reliable and cost-effective sources or alternative water supplies, including opportunities for groundwater recharge, water reclamation and exchanges and transfers.
- CF9.4 **Interdepartmental Coordination.** Coordinate with LADWP to expand, upgrade, or improve the local water distribution system within Sylmar when needed to accommodate increased demand for water.
- CF9.5 **Surplus Properties.** Prior to the disposition or sale of any City-owned property located within the watershed, the department with jurisdiction over said property should consider transfer jurisdiction and control to another City agency, such as Los Angeles Recreation and Parks, that would prioritize the land for multi-benefit projects to include best management practices for the capture and infiltration of stormwater that will aide in recharging the underground water basin, thereby retaining the land for public use and enjoyment.

Wastewater

The City of Los Angeles Department of Public Works Bureau of Sanitation (LABS) provides sewer infrastructure and wastewater treatment services to the City. The primary responsibility of the LABS is to collect, clean and recycle solid and liquid waste generated by residential, commercial and industrial users. The Bureau manages and administers three primary programs: 1) wastewater collection, conveyance, treatment, and disposal; 2) solid waste resources collection, recycling and disposal; and 3) watershed protection. The solid waste resources and watershed protection functions of the Bureau of Sanitation are addressed in the following two sections of this chapter, entitled Solid Waste and Stormwater.

In order to comply with the State Waste Discharge Requirements, a Sewer System Management Plan (SSMP) is prepared for each of the City's sanitary sewer systems to control and mitigate all sewer overflows. The City's wastewater service area consists of two distinct drainage basin areas: the Hyperion Service Area (HSA) and the Terminal Island Service Area (TISA). The HSA covers approximately 515 square miles and serves the majority of Los Angeles, including the San Fernando Valley. TISA covers approximately 18 square miles and serves the Los Angeles Harbor area. Overall, the wastewater system comprises more than 6,500 miles of sewer pipelines connected to the City's four wastewater treatment and water reclamation plants, and 47 pumping plants that have the ability to process over 550 million gallons of flow each day citywide.



The Donald C. Tillman Water Reclamation Plant processes approximately 80 million gallons of waste per day as it purifies wastewater and produces recycled water. Photo Credit: You-are-here.com

The Hyperion Treatment Plant in Playa del Rey, the City’s largest facility, serves more than two-thirds of Los Angeles. The community of Sylmar and the San Fernando Valley is also served by the Donald C. Tillman Water Reclamation Plant in the Sepulveda Basin. The Los Angeles-Glendale Water Reclamation Plant across the freeway from Griffith Park serves the eastern San Fernando Valley communities and the Terminal Island Treatment Plant in San Pedro serves the Los Angeles Harbor area communities. Table 5-3, Wastewater Collection and Treatment Facilities, shows the collection and treatment facilities currently operated by the LABS.

Wastewater generated from Sylmar is conveyed through municipal sewage infrastructure to the Hyperion Treatment Plant, where sewage sludge is removed from the wastewater prior to being treated. The process reclaims significant amounts of beneficial materials from the wastewater and produces water that can be used for industrial, landscape, and recreational uses.

TABLE 5-4

Wastewater Collection and Treatment Facilities

Wastewater Treatment Facilities	Service Area	Location
Hyperion Treatment Plant (HTP)	West, Central Communities	Playa del Rey
Terminal Island Water Reclamation Plant (TIWRP)	Harbor communities	San Pedro
Donald C. Tillman Water Reclamation Plant	San Fernando Valley	Sepulveda Basin
Los Angeles-Glendale Water Reclamation Plant	East San Fernando Valley	Griffith Park

*Source: City of Los Angeles, Department of Public Works Bureau of Sanitation, 2006.

The City of Los Angeles December 2006 Integrated Resources Plan (IRP), prepared by LABS and LADWP, is an integrated approach to address the City’s wastewater, stormwater management and recycled water needs. The IRP identifies the “gaps” between the projected flows and the current system capacities and proposes options to address the identified gaps. The IRP indicates that more treatment capacity is needed due to increased flows and runoff, but rather than adding a brand new treatment plant, it is more cost effective and less disruptive to increase treatment capacity at the existing plants. The IRP identified some general potential locations for a new treatment facility in the San Fernando Valley, southeast downtown, and the western portion of the City.

Goal CF10: **An adequate and reliable wastewater collection and treatment system that supports existing and planned development.**

- CF10.1 **Wastewater Minimization.** Require that wastewater flows be minimized in existing and future developments through stricter water conservation measures, recycling efforts and other features that reduce on-site wastewater output.
- CF10.2 **On-Site Wastewater Output.** Promote advanced waste reduction and diversion methods for all wastewater treatment, including the establishment of methane recovery facilities and the implementation of waste-to-energy projects where characteristics meet criteria for effective energy generation.
- CF10.3 **Recycled Water.** Promote the use of recycled water for non-drinking and irrigation purposes in new industrial and commercial developments.

Solid Waste

The Los Angeles Department of Public Works Bureau of Sanitation (LABS) provides refuse, recyclables, and yard trimmings collection, sewer and storm drain maintenance and repair, wastewater collection and treatment, and many other related services to the residents of single-family and small multi-family households in Los Angeles. Private hauling companies collect other refuse, including most multi-family and all commercial and industrial waste. The City of Los Angeles Solid Waste Management Policy Plan (SWMPP) is the current long-range solid waste management policy plan for the City. The Solid Waste Integrated Resources Plan (SWIRP) seeks to achieve zero waste in Los Angeles.

The Bureau of Sanitation administers the City's Solid Resources Program which includes the collection, recycling, and disposal of over 1.7 million tons per year of solid waste, green waste, recyclables, yard trimmings, bulky items, and other special solid resources materials from residents city-wide. This program also manages contracted recycling programs for apartments as well as commercial businesses which include the recycling and disposal of household hazardous waste, the development of long-term alternatives to landfill disposal, and clean fuel programs related to solid waste. Solid waste facilities include: refuse collection yards; mulching/composting facilities; permanent Solvents, Automotives, Flammables and Electronics (SAFE) centers for household hazardous waste; landfills; and transfer stations. The Central Los Angeles Recycling and Transfer Station (CLARTS) provides the City with an important facility for managing its waste. A list of existing solid waste facilities in the City is shown in Table 5-4, Existing Citywide Solid Waste Facilities.



Private companies offer additional solid waste services in Sylmar.

TABLE 5-5
Existing Citywide Solid Waste Facilities

Facility Name	Type	Community Plan Area
Sunshine Canyon	Landfill	Granada Hills-Knollwood
East Valley South Central North Central Western Harbor West Valley	Yard	Sun Valley-La Tuna Canyon Boyle Heights Northeast Los Angeles West Los Angeles San Pedro Northridge
East San Fernando Valley Westwood UCLA East Los Angeles Hyperion Plant Harbor Area Los Angeles-Glendale	SAFE Centers	Sun Valley-La Tuna Canyon West Los Angeles Central City North Westchester-Playa del Rey San Pedro Northeast Los Angeles
BFI-Falcon Central L.A. Recycling Bradley Transfer & Recycling	Transfer Stations	Wilmington-Harbor City Central City North Sun Valley-La Tuna Canyon
City Fibers City Fibers West Valley Sun Valley Paperstock	Material Recovery Facilities	Mission Hills-Panorama City-North Hills Sun Valley-La Tuna Canyon West Adams-Baldwin Hills-Leimert
Lopez Canyon Landfill Griffith Park Composting Harbor Yard Trimmings Facility	Yard Trimming Processing	Sun Valley-Tujunga-Lake View Terrace- Shadow Hills-East La Tuna Canyon Hollywood San Pedro

*Source: City of Los Angeles, Bureau of Sanitation, 2007.

The Bureau of Sanitation's Five-Year Solid Resources Capital Improvement Program (SRCIP) includes the upgrade and improvement of existing facilities as well as the design and construction of new facilities that support the Solid Resources Program. These facilities include refuse collection yards, mulching/composting facilities, SAFE centers, and transfer stations. In accordance with state regulations, the City has closed the following five landfill facilities: Bishops Canyon, Branford, Sheldon-Arleta, Toyon Canyon, and Lopez Canyon. There are no solid waste facilities located within Sylmar, however there are several facilities within close proximity that provide service to the community. These facilities include the Lopez Canyon Environmental Center, the Bradley Transfer Station and Materials Recycling Facility, and the Sunshine Canyon Landfill.

The Lopez Canyon Environmental Center is a City-owned and operated composting facility located just southeast of the Sylmar boundary at 11950 Lopez Canyon Road in the community of Sunland-Tujunga-Lake View Terrace-Shadow Hills-East La Tuna Canyon. Since 2004, this solid waste facility has been processing curb-side collected yard trimmings from the East San Fernando Valley area and horse manure collected by the City, into valuable mulch and compost. In addition to providing a much needed recycling facility to the residents of this part of the city, mulch generated here is provided free of charge to residents of the local community.

The Bradley Transfer Station and Materials Recycling Facility owned and operated by Waste Management, Inc. is located within the nearby community of Sun Valley-La Tuna Canyon and provides the City with an important resource for managing its waste. This facility receives, sorts, consolidates and prepares municipal solid waste and commercial/residential recyclable materials for transport to other regional landfills and recycled materials processing facilities.

The Sunshine Canyon Landfill, located in Granada Hills-Knollwood, is situated on both City and County land and is privately-owned and maintained by Republic Services, Inc. This landfill provides solid waste disposal and operates recycling and buy-back services to help increase recycling among the residents and businesses of the City and County of Los Angeles.

Goal CF11: Provision of a cost-effective and environmentally sound solid waste management system that protects public health, safety, and natural resources.

- CF11.1 **Waste Reduction.** Promote advanced waste reduction and diversion methods for all solid waste treatment, including the establishment of methane recovery facilities and the implementation of waste-to-energy projects where characteristics meet criteria for effective energy generation.
- CF11.2 **On-site Recycling.** Promote the inclusion of on-site facilities for recycling and waste reduction in single-family, multiple-family, commercial and industrial development projects that support the transformation of waste disposal into resource recovery and economic development opportunities.
- CF11.3 **Recycled Materials.** Encourage recycling of construction material, both during construction and building operation, and the dismantling and reuse of materials rather than demolition and dumping.
- CF11.4 **Interdepartmental Coordination.** Assist the Bureau of Sanitation in finding suitable sites for new solid waste facilities in Sylmar, if necessary, addressing environmental justice issues.

Stormwater

The primary agencies that share flood control responsibilities within the City of Los Angeles are the Los Angeles Department of Public Works, the United States Army Corps of Engineers (Army Corps), the Los Angeles County Department of Public Works (County), and Caltrans. Each agency exercises jurisdiction over the flood control facilities they own and operate.

The Watershed Protection Division of the Department of Public Works, Bureau of Sanitation is responsible for the development and implementation of the Stormwater Program within the City. The Stormwater Program has two major elements — pollution abatement and flood control, and is focused on the control and elimination of stormwater pollution through compliance with the National Pollutant Discharge Elimination System municipal stormwater permit requirements. In addition, general public outreach and education are geared toward increasing the knowledge about the impacts of stormwater pollution and changing the behavior of waste disposal.

Storm drains within the City are constructed by both the City and the Los Angeles County Flood Control District (LACFCD), and managed by the Los Angeles County of Public Works. The storm drain system is comprised of a vast network of natural and constructed channels, debris basins, pump plants, underground pipes and catch basins designed to handle an excess of water as a result of flooding or heavy rainfall. During these events, urban runoff drains from the street, into the gutter, and enters the system through an opening in the curb known as a catch basin. Catch basins serve as the neighborhood entry point to the journey into the ocean. The storm drain system receives no treatment or filtering process, and is completely separate from Los Angeles' sewer system.



The Pacoima Wash and surrounding area provide great opportunities to replenish water supply and link the community with open space.

The overall approach to managing stormwater runoff involves both neighborhood and regional solutions, including source control as a method of reducing pollutants flow to receiving waters. Neighborhood solutions include the use of bio-retention areas, on-site percolation, and neighborhood recharge. Potentially, storm water runoff could be captured by direct percolation through parkways, parking lots with permeable surfaces or in recharging catch basins. Alleys, found in many areas of the city and often underutilized or suffering from environmental degradation, can be redesigned and resurfaced with permeable materials, such as porous paving, to become “green alleys” that provide a range of benefits while still allowing auto access, including the infiltration of stormwater runoff. Overall benefits of these solutions may include increased potable water supply, reduction in storm water pollution and beach contamination, and alleviation of potential for flooding.

Improvements in the capture of stormwater can strengthen local water supplies, helping to reduce dependence on more expensive and energy-intensive sources of water. In the more semi-rural suburban areas, like Sylmar, there are greater opportunities to capture stormwater runoff. In these areas, runoff can be slowed, thereby allowing the stormwater a greater chance to soak into the soil, replenishing both surface moisture levels and underground water tables, and potentially reducing the flood hazard caused by the rapid flow of runoff into the stormwater catch basins and channels.

The groundwater basins in the watershed are critical to local water supply. Sylmar’s location within the Upper Los Angeles River Watershed, relatively flat terrain and “sea of trees”, and its adjacency to the San Gabriel Mountains with heavy rainfall, provide tremendous opportunity to capture stormwater runoff and recharge the natural underground basin levels within the Sylmar Underground Basin.

Stormwater

When stormwater accumulates from precipitation and flows on the ground over impervious surfaces (such as driveways, sidewalks, and streets) it becomes what is known as stormwater runoff and is prevented from naturally percolating into the groundwater table. Stormwater runoff that is not absorbed into the ground is conveyed to coastal waterways by man-made conduits and drains, and could contain debris, chemicals, dirt, and other pollutants.



Channelized portion of the Pacoima Wash near Foothill Boulevard, looking east toward the I-210 freeway.

Goal CF12: **Provision of a storm drainage system that reduces the flow of stormwater and protects water quality, and recharges the groundwater basin by employing watershed-based approaches and best management practices that balance environmental, economic and engineering considerations.**

- CF12.1 **Watershed Revitalization.** Promote watershed management policies that integrate flood protection with water conservation, improve the quality of stormwater runoff and groundwater, and reduce the pollution of water resources while preserving and creating recreation and habitat areas.
- CF12.2 **Local Water Resources.** Optimize local water resources to reduce water dependence on imported water by improving groundwater infiltration, facilitating on-site collection systems for stormwater and graywater, maximizing the capture and reuse of stormwater runoff, and integrating groundwater infiltration with other public and/or beneficial uses.
- CF12.3 **Groundwater Infiltration.** Encourage the incorporation of bio-retention facilities and the use of permeable materials for the paving of sidewalks, driveways, and parking areas, when feasible, and the day lighting of buried streams and other policies which promote stormwater infiltration.
- CF12.4 **Flood Control Channels.** Support a watershed-wide network of pedestrian, equestrian, and bicycle facilities along the flood control channels.
- CF12.5 **Railroad-right-of-ways.** Enhance railroad right-of-ways to increase flood protection, provide trails and open spaces, create swales for stormwater capture, improve water quality and restore wildlife habitat.
- CF12.6 **Interdepartmental Coordination.** Support the development of a new comprehensive flood management plan for the watershed through coordination among City departments.

Energy

The Los Angeles Department of Water and Power (LADWP) provides electric service to the City of Los Angeles. To ensure a reliable supply of power, the LADWP maintains a diversified energy generation mix – including coal, natural gas, large hydroelectric, nuclear, and renewable power, such as wind, biomass, solar and cogeneration. The LADWP draws its energy supply from in-basin power plants and several out-of-state facilities in Nevada, Utah and the Pacific Northwest. Business and industry consume about 70 percent of the electricity generated, while residents constitute the largest number of consumers. In addition to serving these consumers, the LADWP generates electricity to light public streets and highways, powers the City’s water system, and sells electricity to other utilities.

The LADWP’s 2010 Power Integrated Resource Plan (IRP) is the planning document that provides a framework for addressing the future energy needs of the City. This plan focuses on renewable power, greenhouse gas reduction, and energy efficiency. One of the key policy areas highlighted in this plan is to ensure that the power generation, transmission and distribution infrastructure operates in a reliable and efficient manner. The LADWP initiated a Power Reliability Program in 2007 to improve maintenance practices, address the aging power system infrastructure and increase capital programs necessary to support growth.



High voltage powerlines transmit electricity from the Pacific Northwest to the Los Angeles Region.

The LADWP operates local steam plants, hydroelectric plants and power plants which generate electricity to be transmitted through an extensive network of receiving stations, distribution stations, overhead transmission lines, and underground distribution lines located throughout the service area of the LADWP. All receiving stations are connected to the “belt line” that extends throughout the City and supplies power to them as required. These receiving stations transform these high voltages for distribution to the distributing stations and to individual customers. Distributing stations generally have a two-mile radius, with an average of one per year added to the system citywide. Table 5-5, LADWP Generation Resources, shows a list of the City’s power generation resources.

TABLE 5-6

LADWP Generation Resources

Type of Resource	Facility Name	Location
Basin Thermal Generation (natural gas-fueled generation stations)	Harbor Haynes Scattergood Valley	Los Angeles, CA (Wilmington) Long Beach, CA Playa del Rey, CA Los Angeles, CA (Sun Valley-La Tuna Canyon)
Coal-Fired Thermal Generation	Navajo Intermountain	Page, AZ Delta, UT
Nuclear-Fueled Thermal Generation	Palo Verde Nuclear Generation Station	Phoenix, AZ
Large Hydroelectric Generation	Castaic Pumped Storage Power Plant Hoover Power Plant	Castaic, CA Hoover Dam, NV
Small Hydroelectric Plants	Owens Gorge Owens Valley Los Angeles Aqueduct	Owens Valley, CA Owens Valley, CA Los Angeles, CA (Sylmar)

* Source: LADWP, 2007 Integrated Resources Plan. (Need to update per 2010 plan)

The LADWP owns and operates several facilities within the boundaries of Sylmar that are integral components of the City's electrical distribution system. These facilities include the Sylmar Converter Station, a network of overhead and underground electrical distribution lines, and 4 receiving/distribution stations that transmit power through electric cables that lead through the City of Los Angeles to the Pacific Ocean.

The Sylmar Converter Station is the southern terminal of the 846-mile Pacific Direct Current Intertie, which is the electric power transmission line that transmits electricity from the Pacific Northwest to the Los Angeles area using high voltage direct current (HVDC). Originally built in 1970, the Sylmar Converter Station was expanded into two facilities (Sylmar East and Sylmar West) in the mid-1980s to handle the growing energy load. In 2005, the partners of the Sylmar Converter Station (which include the LADWP, Southern California Edison and the Cities of Burbank, Glendale, and Pasadena) rededicated the Sylmar East Converter Station after an extensive modernization process which involved upgrading outdated technology, consolidating the East and West facility into one location, and increasing its capacity in order to carry the entire station load. This modernization project has helped increase the reliability and lifespan of the power system as the upgrades reduce the operation and maintenance costs, increase energy efficiency, and eliminate the use of mercury.

Goal CF13: An adequate, safe, and orderly supply of electrical energy to provide for the existing and future land uses of Los Angeles.

- CF13.1 **Utility Easements.** Protect the use of public utility easements, right-of-ways, and land set-asides to ensure adequate electrical facilities for current and future demand.
- CF13.2 **Electrical Facilities.** Work with LADWP to ensure that adequate electrical facilities are available to meet the demand of existing and future developments and conservation techniques are integrated into new and existing development projects.
- CF13.3 **Facility Design.** Support the construction of well-designed power system facilities, including receiving and distribution stations, so that they are in harmony with the surrounding neighborhood.
- CF13.4 **Undergrounding Utilities.** Provide for the undergrounding of new and existing electrical distribution lines unless it is determined not to be economically or practically feasible as a result of significant environmental constraints.
- CF13.5 **Green Technology.** Support efforts to promote the use of clean, renewable energy that is diverse in technology and location to decrease dependence on fossil fuels, reduce emissions of green house gases, and increase the reliability of the City's power supply.

Goal CF14: **Shared use of utility land and right-of-ways for multiple environmental, public health, and community benefits.**

- CF14.1 **Easements and Right-of-Ways.** Consider opportunities for the establishment of new pocket parks, recreation areas and trails that provide public access through the easements and right-of-ways of the flood control channels and the transmission lines, linking large portions of the watershed to other areas throughout the community.
- CF14.2 **Runoff Capture.** Encourage the capture and infiltration of stormwater along existing power line easements for groundwater recharge, water quality benefits, and habitat restoration opportunities.
- CF14.3 **Local Agriculture.** Support easements and rights-of way leases for commercial nursery and other agricultural businesses, community garden plots, and farmer's markets.

Street Lighting

The Bureau of Street Lighting (BSL) in the Department of Public Works is responsible for the design, construction, operation, maintenance and repair of the City's Street Lighting System. It is the City policy that all new street projects include sidewalk, street trees, and street lights, unless unusual circumstances exist. The BSL maintains standards to ensure that street lighting installed in the City is designed to meet National Lighting levels that reduce sky glow and glare. The BSL maintains approximately 220,000 streetlights within the City which are tested for efficiency, safety, and maintainability prior to installation. These streetlights are classified as local, collector, or major/arterial representing the type of lighting used for the different classifications of roadways.

Street lighting serves many roles in a city of the size, complexity, and history of Los Angeles. The main purpose is to allow for safe and comfortable vision during the night on public streets and sidewalks with benefits that include the reduction of automobile accidents, the facilitation of traffic flow, the promotion of nighttime business operation, and provides an increased sense of personal safety and security for the public. While street lighting enhances community safety relative to crime prevention and feelings of well-being, it also contributes to the architectural, cultural, or historic character of the community.



Street lighting systems provide for road safety and security benefits, which are vital elements for navigable, safe and successful communities.

Street lighting is not publicly financed in the City but is the direct financial responsibility of the owners of adjoining properties, who are considered to directly benefit from street lights. The City's policy for financing street lighting requires adjoining property owners to bear the annual cost of operation and maintenance through a special assessment levied against each property which benefits from the Street Lighting System. BSL is also responsible for the financial administration of the Lighting District. Proposition 218 was passed by the votes of California in November 1996, and requires a vote of property owners in order to impose new or increased assessments for street improvements installation, or maintenance of such improvements, including street lighting, sidewalks, sewers, street paving, trees, and landscaping.

Goal CF15: Provision of a street-lighting system that protects and preserves the nighttime environment and contributes to appropriate levels of lighting for streets, parking areas, and pedestrian areas, with minimum impact on the environment and adjoining properties.

- CF15.1 **Energy Management.** Ensure efficient and effective energy management while providing appropriate levels of lighting to meet safety needs.
- CF15.2 **Lighting Integration.** Ensure that street lighting designs meet minimum standards for quality lighting to provide appropriate pedestrian visibility for usage of streets and sidewalks in commercial centers and neighborhood districts, and enhance the pedestrian oriented character of these districts.
- CF15.3 **Assessment Districts.** Coordinate efforts between the community and the BSL to establish new Street Lighting Assessment Districts in the older areas of the community in need of new or updated street lighting infrastructure.



Implementation

The Sylmar Community Plan is a comprehensive and long-range document that expresses a vision for the future and guides how that vision is implemented through private and public development. The Community Plan identifies policies that are limited to authorities that can be implemented under the jurisdiction of the City of Los Angeles (all departments) and does not prescribe actions of other agencies, such as the City's school districts. In addition, implementation of many of the policies may require joint action with external governmental agencies, such as the California Department of Transportation, the school districts, water service providers, the Los Angeles County Local Agency Formation Commission (LAFCO), and others.

A variety of ordinances, programs, and decisions made by the City in regard to discretionary and non-discretionary building projects are employed to implement the Plan. City actions on discretionary projects involving land use require a finding that the action is consistent or in conformance with the General Plan. City Planning Department decision makers, such as hearing officers and zoning administrators, refer to the Community Plan text and the Land Use Map of the Community Plan when writing findings on land use decisions.

Implementation programs are mechanisms put in place to ensure that Sylmar Community Plan goals and policies are realized. This chapter discusses how the community plan policies and programs are implemented in land use decision making. The chapter details the programs, shown in a series of tables, which implement the goals and policies found throughout the Community Plan.

The Implementation Process

The goals and policies set forth in Chapters 3, 4, and 5 of the Sylmar Community Plan are implemented through a variety of mechanisms, including regulation and development review; financing and budgeting; and inter-departmental and inter-governmental coordination.

Many Community Plan policies are implemented through regulations, such as zoning, adopted by the City based on the City's "police power" to protect the public health, safety, and welfare of its citizens. City ordinances also create a development review process that provides for City review of individual project proposals and authorizes the City to approve, deny, or condition projects based on their consistency with the Community Plan. Some development review programs, such as the California Environmental Quality Act (CEQA), are ongoing and will continue to be used as a tool for land use decision making. Other programs are implemented at the time of Community Plan adoption, such as zone changes, supplemental use districts, and design overlays.

Coordination among City departments is critical to the successful implementation of many Community Plan policies, such as street reclassifications, park planning, and streetscape improvements. While the Community Plan policies and implementation programs are limited to authorities that can be implemented under the jurisdiction of the City of Los Angeles, implementation of some Plan policies may also require coordination and joint actions with numerous local, regional, state, and federal agencies. These agencies provide services, facilities, or funding and administer regulations that directly or indirectly affect many issues addressed in the Community Plan. These external governmental agencies, such as the California Department of Transportation (CALTRANS), the Los Angeles Unified School District, water service providers, the Los Angeles County Local Agency Formation Commission (LAFCO), among others, also look to the Community Plans for their planning and guidance in decision making.

Amendments to the Community Plan

Changes to the Community Plan may be proposed from time to time to address changing conditions, new opportunities, and unforeseen circumstances. As such, the Community Plan must be a living, flexible document, allowing for changes that ultimately assist in enhancing and implementing the community's vision. It is necessary, therefore, to establish a fair, orderly, and well defined process to govern how amendments occur.

Community Plans are part of the City's General Plan and thus any changes to the Community Plan are considered General Plan amendments. Amendments may propose a change in the land use designation for a particular property or changes to the Community Plan's policies and text. Private requests are subject to an established public review and approval process. Although applicants have the right to submit amendment requests to the City, not all requests merit study and consideration. The amendment process allows for the City to deny a proposed amendment if it is inconsistent with the goals and policies of the Community Plan.

Specific Implementation Programs

Some recommendations of the Sylmar Community Plan are enacted concurrently with adoption of the Plan. These include modified street standards, plan amendments and footnotes, zone changes (including the removal of, addition to or change in qualifying conditions), and height district changes. These are included in Tables 6-1, 6-2, and 6-3. Implementation programs are identified with a distinct number (i.e. P1), followed by a program description and a list of the policies that the program implements

Detailed information on recommendations enacted concurrent with Plan adoption can be found in the following documents:

Case File CPC-2006-5569-CPU

- Matrix of Existing, Planned and Proposed Land Use for the Sylmar Community Plan Update
- Land Use and Zone Change Map
- Sylmar Community Plan Implementation Overlay (CPIO)
- Sylmar Equinekeeping "K" District
- Modified Street Standards

Specific implementation programs for the Sylmar Community Plan are organized into three sections:

- Land Use and Urban Design
- Mobility
- Community Facilities and Services

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P1	General Plan Land Use Designations. The Plan designates land for various housing densities, as shown on the Land Use Map.	All Residential	LU1.1	DCP, LADBS
P2	Housing Variety. The Plan promotes greater individual choice through: a) its allocation of lands for a variety of residential densities; and b) its promotion of housing in mixed-use projects at major intersection nodes and transit-oriented neighborhood near the Sylmar Metrolink Station.	All Residential	LU1.1	DCP
P3	Single-Family Housing. The residential neighborhoods of the Plan area are located within a variety of geographic settings that have facilitated the production of numerous housing typologies across several development periods and that continue to serve a diversity of income types.	All Residential	LU1.1	DCP
P4	Consistency between Land Use and Zones. The Plan enacts zone changes and plan amendments to preserve the existing stable residential areas as shown on the Plan Map.	All Residential	LU1.2, LU2.2	DCP
P5	Community Gardens. Consistent with the LAMC, allow community gardens as a use in a variety of zones, create specific “community garden” zoning regulations and encourage measures that protect gardens from confiscation.	All Residential	LU1.3	DCP
P6	Quimby Program. This policy shall be implemented pursuant to LAMC Section 17.12, whereby most residential development projects requesting a subdivision or a zone change where required, as a condition of approval of the project, to either dedicate land for recreation and park purposes, or pay a fee in-lieu (Quimby Fees). The in-lieu fee is calculated on a per unit (for condominiums) or per lot basis, with the amount of fee dependent on the zoning of the property.	All Residential	LU1.3	DCP, LARAP
P7	Land Lease Communities. The Plan preserves existing land lease communities (e.g. manufactured housing and mobile home parks) by retaining the RMP zone. Future development of these sites shall be consistent with the Plan. City has limited regulatory control over land lease communities, their density, or anything on the interior of the property.	All Residential	LU1.4	DCP
P8	Affordable Housing and Displacement. In all discretionary actions, the decision maker may adopt a finding which addresses any potential displacement of residents as part of any decision relating to the new housing construction.	All Residential	LU1.5	DCP, LAHD
P9	Housing Distribution, Affordability, and Senior Housing. The Plan designates specific areas for Medium Residential density development and encourages mixed-use development in transit-oriented and mixed-use corridors which would allow for a mix of income levels and multi-family residential housing, as well as for senior citizen housing. Continue the implementation of the Density Bonus program in order to facilitate a mix of income level residential units within the community.	All Residential	LU1.5	DCP, LAHD, LADBS
P10	Low III Clustered Development. The Plan includes various areas designated Low III density residential which allow corresponding zones of RD5 and RD6 which permit clustering.	All Residential	LU1.5	DCP

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P11	Multi-Family Residential Areas. The Plan identifies specific areas throughout the community where multi-family residential development is permitted. The Plan designates land as Medium and Low Medium I and II residential, with corresponding zoning, for multiple-family residential developments.	All Residential	LU1.6	DCP, LADBS
P12	Residential Design Guidelines. By-right residential projects should be encouraged to adhere to all applicable adopted Citywide and Community Plan design guidelines and the decision maker for discretionary projects shall adopt conditions that require adherence to such adopted guidelines.	All Residential	LU2.1, LU2.3, LU2.4, LU10.2-10.3, LU11.1-11.8	DCP, LADBS
P13	CPIO Design and Development Standards. The Community Plan Implementation Overlay (CPIO) establishes and refines design and development standards for parcels within the Overlay. The following are regulated by the CPIO: land uses, ground floor retail, building height and transitions, setbacks, circulation, fences, signage, utilities, and landscaping. See Sylmar CPIO.	All Residential, Commercial, and Industrial	LU2.1-2.4, LU10.2-10.3, LU11.1-11.8, LU12.2, LU12.5, LU13.1-13.4, LU14.1-14.13, LU15.1, LU15.8, LU17.1-17.5, LU22.1-22.7	DCP, LADBS
P14	Infill Development and Small Lot Development. The Plan establishes height limits and amends land use designations and corresponding zones to implement this policy. In addition, guidelines that require infill residential development to complement existing scale, massing, setbacks, and character and are compatible with architectural styles in stable single-family neighborhoods are included in the Plan.	All Residential	LU2.2-2.4	DCP, LADBS
P15	Baseline Mansionization and Hillside Ordinances. Continue further implementation of the Citywide Baseline Mansionization and Hillside Ordinances and implement conformance with any applicable Community Plan design guidelines.	All Residential	LU2.2-2.3	DCP, LADBS
P16	Transitional Height Standards. Where applicable, Section 12.21.1 A-10 of the LAMC (transitional height) shall apply to all projects.	All Residential	LU 2.2, LU2.3, LU11.3, LU11.6	DCP, LADBS
P17	Utility Design. The Plan encourages locating electrical equipment, trash enclosures, roof-top and other utility equipment away from street view and screened by landscaping for aesthetic appeal.	All Residential	LU2.4	DCP, LADBS
P18	SurveyLA Findings - Eligible Historic Resources. The findings of the Los Angeles Historic Resources Survey (SurveyLA) identify potential historic resources and promote the protection and enhancement of the area.	All Residential and Commercial	LU3.1, 15.2	DCP
P19	Preservation Tools. The Plan supports the establishment of a Historic Preservation Overlay Zone or other overlay district designations, as warranted, in order to protect structures of historic significance in these areas or otherwise conserve desirable neighborhood character.	All Residential and Commercial	LU3.1, LU15.2	DCP
P20	Rural Residential Character. The Plan emphasizes residential compatibility for all new development to ensure that the semi-rural equestrian lifestyle is reflected in the design of the project.	All Residential	LU3.2	DCP

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P21	Sylmar Trail Network. The Plan includes a Trail Network map identifying sides of the streets where trails and horse crossings are planned. Projects adjacent to an identified trail shall make improvements to develop and/or connect the Trail Network, where feasible. By-right projects should be encouraged to adhere to all applicable adopted Community Plan design guidelines and the decision maker shall adopt conditions that require adherence to such adopted guidelines.	All Residential and Commercial	LU3.3-3.4, LU13.6	DCP, LADBS, PW
P22	Sylmar "K" District. The amended Sylmar Equinekeeping "K" Supplemental Use District includes additional lot conditions to further support equinekeeping lots and the development of trails. Projects located within the "K" District shall comply with minimum lot size standards, distance requirements, and lot conditions. Projects adjacent to an identified trail and located within the "K" District shall also develop and/or improve the trail network, as shown in the Community plan, Figure 4-5, Trail Network. See Sylmar "K" District.	All Residential	LU3.3-3.4, LU4.2, LU5.1, LU5.3-5.4	DCP, LADBS
P23	Preservation of Equinekeeping Areas. The Plan amends the established Sylmar Equinekeeping "K" Supplemental Use District and implements zoning and land use designations to preserve these areas.	All Residential	LU4.1-4.2	DCP
P24	Agricultural Lot and Open Space Preservation. The Plan maintains low density land use designations and zoning in areas along the foothills and near open spaces.	All Residential	LU4.3	DCP
P25	Legal Non-Conforming Equinekeeping Lot Protection. The Plan recognizes that certain areas within the Equinekeeping "K" Supplemental Use District shall have the right to maintain equinekeeping uses despite not meeting the minimum lot size requirements.	All Residential	LU4.4	DCP, LADBS
P26	Expansion of Equinekeeping "K" District. The Plan proposes to expand the existing Equinekeeping "K" Supplemental Use District in Sylmar to include areas that currently meet the lot size criteria and maintain equinekeeping uses. The Plan further supports the expansion or creation of new "K" Districts where appropriate and feasible.	All Residential	LU4.5	DCP
P27	Transitions Adjacent to Equinekeeping Lots. By-right residential projects adjacent to equinekeeping lots should be encouraged to adhere to all applicable adopted Citywide and Community Plan design guidelines and the decision maker for discretionary projects shall adopt conditions that require adherence to such adopted guidelines.	All Residential	LU5.2	DCP, LADBS
P28	Notification of Future Residents. Require discretionary projects to notify future residents that animal keeping uses are permitted in adjacent lots.	All Residential	LU5.5	DCP
P29	Hillside Development. Continue the implementation of the Citywide Hillside Ordinance and implement conformance with any applicable design standards identified in the Citywide Baseline Hillside Ordinance as applicable.	All Residential	LU6.1-6.8	DCP, LADBS
P30	Slope Density. Any development in hillside areas with average natural slopes in excess of 15 percent shall be limited to the minimum density housing category for purposes of enforcing the slope density formula of the LAMC 17.05.	All Residential	LU6.1, LU6.3	DCP, LADBS

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P31	Ridgelines. Study and/or adopt a Ridgeline Ordinance that preserves the contours of natural ridgelines will help to implement this policy.	All Residential	LU6.5	DCP
P32	Watershed Management. Require the incorporation of bio-retention facilities and use of permeable materials for the paving of sidewalks, driveways, and parking areas when feasible.	All Residential	LU7.2	DCP, LADOT, PW
P33	"Green" Building. The Department of Water and Power and other water consumption and energy conservation programs, which provide technical assistance and referral information regarding resources, provide technical assistance and disseminate information and guidelines to residential property owners and developers to encourage energy efficient residential building site and landscape design utilizing resources such as LEED, the California Green Building Code as well as any applicable LAMC Green Building standards and guidelines.	All Residential	LU7.1-7.4	LADWP, LADBS
P34	Preserve Neighborhood Character. The Plan Map identifies parcels where only single-family residential development is permitted; it protects these areas from encroachment by designating them as Low Density Residential (Minimum, Very Low I, Very Low II, Low I, Low II, and Low III).	Single-Family Residential	LU8.1-8.2	DCP, LADBS
P35	Low I, II, and III. The Plan divides the Low Residential (RE9, RS, R1, RD5, and RD6) land use designation into three categories: Low I (RE9, RS), Low II (R1), and Low III (RD5, RD6) to help address neighborhood compatibility and maintain existing character.	Single-Family Residential	LU8.1	DCP, LADBS
P36	Housing Density. The Plan directs more intensive residential development to neighborhoods designated as Low Medium I (R2, RD4, RD5), Low Medium II (RD1.5, RD2) and Medium (R3) Residential and mixed-use and transit-oriented neighborhoods.	Single-Family Residential	LU8.1-8.2	DCP, LADBS
P37	Flag Lots. The Plan discourages precedent setting discretionary actions that allow subdivision of lots into the "flag lot" design.	Single-Family Residential	LU8.3	DCP
P38	Subdivision Tracts, Parcel Maps, and Small Lot Subdivisions. Require all subdivision tracts, parcel maps, and small lot subdivisions to be compatible with surrounding development pattern and overall neighborhood character with respect to density, lot size and width, grading, setbacks, orientation, streetscape, and circulation. The decision maker for discretionary projects shall adopt conditions that require adherence to adopted Citywide and Community design guidelines.	Single-Family Residential	LU9.1-9.8	DCP, LADOT, PW
P39	Multi-family Residential Areas. The Plan Map identifies specific areas along and near major corridors and transit- and pedestrian-oriented areas where multiple-family residential development is permitted. The Plan protects multiple-family residential development from encroachment by designating these sites as Low Medium I, II, Medium and High Medium Density Residential.	Multiple Family Residential	LU10.1	DCP, LADBS
P40	Front Yard Parking. LAMC prohibits parking in front yard setback.	Multiple Family Residential	LU11.7	LADBS

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P41	Protection of Commercial Land. The Plan maintains existing commercial land use designations and strengthens existing zones by establishing CPIO subdistricts that prohibit the establishment of stand-alone single-family development as well as require ground floor commercial space for projects that include multiple-family residential.	All Commercial	LU12.1	DCP, LADBS
P42	Lot Consolidation. Certain residentially-zoned properties within commercial plan designations are recommended to be rezoned to a commercial zone to implement this policy.	All Commercial	LU12.3	DCP
P43	Support Equestrian Related Services. The Plan encourages commercial development within the Equinekeeping "K" Supplemental Use District to incorporate equestrian-related amenities at the time of new development.	All Commercial	LU12.4	DCP
P44	Auto-Related Uses and Services. The Sylmar CPIO establishes and refines design and development standards for parcels within the Overlay. Development of auto-related uses and services are allowed within certain subdistricts within the CPIO. See Sylmar CPIO.	All Commercial	LU12.5	DCP, LADBS
P45	Commercial Compatibility. The Plan brings zones, height districts, land uses into consistency with the use of subject and adjacent properties. Furthermore, the Sylmar CPIO includes standards that establish and refine design and development standards for commercial projects that ensure better neighborhood compatibility by further tailoring the zoning densities and intensities, as well as height limits.	All Commercial	LU13.1-13.4, LU17.1-17.5	DCP, LADBS
P46	Commercial Development. By-right commercial projects should be encouraged to adhere to all applicable adopted Citywide and Community Plan design guidelines and the decision maker for discretionary projects shall adopt conditions that require adherence to such adopted guidelines.	All Commercial	LU13.5, LU14.1-14.13, LU15.1-15.2, LU15.6-15.7, LU16.2, LU17.1-17.5, LU18.1-18.6, LU19.1-19.6, LU19.8, LU20.1-20.5	DCP, LADBS
P47	Foothill Streetscape. By-right projects should provide landscaped parkways that are lined with palm trees and deciduous trees and adhere to applicable adopted Citywide and Community Plan design guidelines. The decision maker for discretionary projects shall adopt conditions that require adherence to this policy and adopted guidelines.	All Commercial	LU15.6	DCP, PW
P48	Eco-Friendly Design. Provide technical assistance and information to property owners and developers to encourage energy efficient residential building and landscape design utilizing resources such as the U.S. Green Building Council's Leadership in Energy and Environmental Design (LEED) certification program, the California Green Building Code as well as any applicable City Municipal Code (LAMC) Green Building standards and guidelines.	All Commercial and Industrial	LU16.1, LU23.3	DCP, LADBS, LADWP
P49	Commercial Landscaping Requirements. Design guidelines in the Sylmar CPIO and Landscaping standards of the LAMC implement this policy.	All Commercial	LU16.2	DCP, LADBS

Table 6-1 Implementation Programs -
Chapter 3 - Land Use and Urban Design

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P50	Prioritize New Infill Development Near Transit. The Plan facilitates the construction of new infill projects close to major bus centers and mass transit stations over other areas through contextual increases to height and development intensity within the major nodes and transit oriented development areas of the CPIO subdistricts.	Transit-Oriented District and Mixed Use Boulevards	LU19.1	DCP
P51	Mobile Home Park. The Plan retains the RMP zone.	Transit-Oriented District and Mixed Use Boulevards	LU19.7	DCP
P52	Parking Structures. Coordinate with other agencies to develop public parking structures in Transit-Oriented and Mixed-Use Districts.	Transit-Oriented District and Mixed Use Boulevards	LU19.9-19.10	DCP, LADOT, Metro
P53	Shuttle Buses. Work with Metro and LADOT to initiate shuttle bus programs to serve transit stations, as funding permits.	Transit-Oriented District and Mixed Use Boulevards	LU19.12	LADOT, Metro
P54	Industrial Land Retention. The Plan Map identifies lands which have existing industrial designations to accommodate the variety of uses and initiates plan amendments and recommended corresponding zone changes to implement this policy. The Plan also enacts zone changes and/or plan amendments to preserve existing industrial areas as shown on the Plan Map.	Industrial	LU21.1-21.2	DCP
P55	Industrial Development. By-right commercial projects should be encouraged to adhere to all applicable adopted Citywide and Community Plan design guidelines and the decision maker for discretionary projects shall adopt conditions that require adherence to such adopted guidelines.	Industrial	LU22.1-22.7	DCP, LADBS
P56	Industrial Buffering and Transitions. The Plan, through plan amendments and corresponding zone changes, establishes transitional buffering between residential and industrial uses.	Industrial	LU22.2-22.3	DCP
P57	Historic Preservation. The Plan includes a list of sites which have been designated as California State Historic Landmarks and Los Angeles Historic-Cultural Monuments. Through this policy, the Plan supports the continued identification and recommendation of appropriate landmarks for inclusion in the list of designated sites.	Historic, Architectural, and Cultural Resources	LU24.1	DCP
P58	Rehabilitation, Restoration, Reuse. This policy is implemented through administration of the City's Historic Preservation Overlay Zone Ordinance, Cultural Heritage Ordinance and compliance with the Secretary of Interior Standards and Guidelines, and any applicable Citywide, Community Plan, and overlay district guidelines.	Historic, Architectural, and Cultural Resources	LU24.4-24.5	DCP
P59	ZIMAS, Navigate LA. Work with other departments to identify and/or map the Sylmar Trail Network to help implement the development of trails, as funding becomes available.	All Residential and Commercial	LU3.3-3.4, LU13.6	DCP, LADBS, PW

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P60	Complete Streets. The Sylmar Community Plan implements "Complete Streets" by adopting a Generalized Circulation System (Figure 4-1), Modified Street Standards (Figure 4-3), and Priority Streets (Figure 4-2) that focus the priorities for street investments on the following modes of travel: pedestrians, bicycles, transit, and motorized vehicles.	General Mobility	M1.1	DCP, LADOT, PW
P61	Mobility for Challenged Users. Identify locations where access may be improved. Develop a priority list of physical improvements and identify potential funding sources. Coordinate CityRide transit services and Los Angeles County ACCESS transit services with social centers.	General Mobility	M1.2	DCP, LADOT
P62	Mobility Enhancements. Continue implementation of the City's Capital Improvement Program Initiatives as identified through the goals and policies of the Plan's Mobility Chapter IV and Community Facilities and Infrastructure Chapter V.	General Mobility	M1.3	DCP, LADOT, PW
P63	Private Investment for Multi-Modal Right-Of-Way Design. Require developments to use the Citywide and Community Plan Design Guidelines in the design of the right-of-way to include automobiles, pedestrians, bicycles, and equestrian/recreational trails.	General Mobility, Bicycling	M1.4, M5.3	DCP, LADOT, PW
P64	Grants for Off-Site Facilities/Amenities. Apply for grants and funding opportunities offered by public agencies such as the Los Angeles County Metropolitan (Metro) and coordinate with other departments once funding is awarded to administer grants.	General Mobility	M1.4	DCP, LADOT
P65	Modified Street Standards. The Plan implements modified street standards as referenced on Figure 4-1, Circulation System and Table 4-2, Street Modifications.	General Mobility, Bicycling	M1.5, M5.4	DCP, LADBS, pw
P66	Streetscapes. The Plan identifies programs, plans, and guidelines which encourage the implementation of streetscapes which introduce traffic calming, street trees, lighting, well-maintained sidewalks with benches, and front porches within residential developments.	General Mobility	M2.1-2.2	DCP, LADOT, PW
P67	Spaces for People, Streetscapes, Gateways. As funding becomes available, implement a Streetscape Plan for San Fernando Road, Maclay Avenue, Foothill Boulevard, and Polk Street. These plans may include specifications for lighting, street trees, benches, plazas, and other amenities in the public right-of-way.	General Mobility	M2.1-2.2	DCP, LADOT, PW
P68	Special Events. Prepare and implement special traffic management plans to mitigate the impact of street closures associated with special events.	General Mobility	M2.3	LADOT, PW, LAPD
P69	Watershed Management. Encourage the incorporation of bio-retention facilities and use of permeable materials for the paving of sidewalks, driveways, and parking areas when feasible. Street Standard Plan S-480-O provides guidance for the design of watershed management in public rights-of-way.	General Mobility	M2.4	DCP, LADOT, PW
P70	Safety for All Users. Develop a priority list of pedestrian crossing improvements through a pedestrian safety audit throughout the community. Include enhanced features such as bulb-outs, landscaped median refuges and audio/visual warnings where appropriate.	General Mobility	M3.1	DCP, LADOT, PW

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P71	Safe School Routes. Collaborate with other agencies to implement Caltrans' "Safe Routes to School" programs.	General Mobility	M3.2	DCP, LADOT, PW
P72	Easements and Right-of-Way for Bicyclist. The Citywide Mobility 2035 implements this policy.	General Mobility	M3.3-3.4	DCP, LADOT
P73	Easements and Rights-of-Way for Trails. Coordinate with other agencies to designate and develop trails, as indicated on the Trails System Map, Figure 4-5 of the Sylmar Community Plan, to compliment and connect to the Rim of the Valley Trail Network.	General Mobility	M3.3-3.4	DCP, LADOT, PW, LARAP
P74	Reclaimed Land for Bikeways. Implement the General Plan Framework Greenways Network and designate, where feasible Class I facilities (bike paths) for those public right-of-way indicated as Greenway Corridors in Figure 6-1 of the GPF and corridors indicated as greenway corridors in the Greenway Network, Figure 5-3 of the Sylmar Community Plan.	General Mobility, Bicycling	M3.5, M5.3	
P75	Reclaimed Land for Trails. Coordinate with other agencies to designate and develop trails, as indicated on the Trail Network Map, Figure 4-5, and Greenway Network, Figure 5-3 of the Sylmar Community Plan, to complement and connect to the Rim of the Valley Trail Network.	General Mobility	M3.5	DCP, LADOT, PW, LA County PW, LADWP, SoCal Edison
P76	Pedestrian-Oriented Development. The enhancement of established pedestrian orientation is implemented through the building and site disposition standards of the CPIO, Citywide and Community Plan specific design guidelines.	Walking	M4.1	DCP, LADBS
P77	Pedestrian Priority Streets. Implement streetscape plans for Pedestrian Priority Streets, as shown on Figure 4-2.	Walking	M4.2	DCP, LADOT, PW
P78	Pedestrian Priority Amenities. New development projects will be required to provide pedestrian amenities as part of their project review within the individual CPIO subdistricts.	Walking	M4.2-4.3	DCP, LADBS
P79	Walkability Checklist. The Walkability Checklist provides guidelines for implementing this policy.	Walking	M4.1, M4.3	DCP
P80	Bikeway Connections. The Plan is consistent with the adopted Citywide Mobility 2035 that identifies and implements several classes of bicycle facilities which provide access to nearby transit, recreation and other public facilities.	Bicycling	M5.1	DCP
P81	Bicycle Priority Streets. The Plan identifies streets for priority uses, including bicycle, in order to guide and prioritize types of improvements for particular streets.	Bicycling	M5.2, M5.4	DCP
P82	Bicycle Amenities. The Plan is consistent with the adopted Citywide Mobility 2035 that identifies and implements several classes of bicycle facilities which provide access to nearby transit, recreation and other public facilities.	Bicycling	M5.5	DCP

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P83	Trail Network. The Plan includes a Trail Network map, as shown on Figure 4-5 of the Sylmar Community Plan, indicating which streets and sides of the street trails and trail amenities should be improved or developed. Products such as rails, located in the public right-of-way shall consult with the Department of Public Works and the Bureau of Engineering for review, testing, and approval.	Trails	M6.1, M6.4	DCP
P84	K District. The Equinekeeping "K" Supplemental Use District requires development adjacent to trails, as shown on the Trail Network map, Figure 4-5 of the Sylmar Community Plan, to develop or improve trails.	Trails	M6.1, M6.4	DCP
P85	Trail Design. The Plan Design Guidelines for Trails include guidelines for trail design and amenities.	Trails	M6.1, M6.4, M6.5, M6.6, M6.9	DCP
P86	Trail Priority Streets. The Plan identifies streets for priority uses, including trails, in order to guide and prioritize types of improvements for particular streets.	Trails	M6.2	DCP
P87	Trail Safety. The Plan Design Guidelines for Trails include guidelines for the design of safe trails. Modified street standards for trails are shown on Figure 4-3 to help guide the development of trails.	Trails	M6.7	DCP
P88	Regional Coordination on Public Land. The Plan includes a Trail Network map, Figure 4-5, with connections between neighborhood trails and open space trails, including the Rim of the Valley Trail network.	Trails	M6.8	DCP
P89	Scenic Highways. Implement the programs delineated within the Scenic Highways Plan of the City's General Plan with regard to all designated scenic highways within the Sylmar Community Plan area (also referenced through applicable policies in Chapter 3).	Scenic Highways	M7.1	DCP, LADOT, CalTrans
P90	Study. In addition to the adopted Scenic Highways in the Mobility Plan 2035 of the City's General Plan, the following future study corridor should be explored for possible designation as a scenic highway: Maclay/Harding/Pacoima Canyon Road.	Scenic Highways	M7.1	DCP, LADOT
P91	Mobility Plan 2035. The Mobility Plan 2035 of the City's General Plan identifies scenic highways, as shown on Map E of that Element. Scenic Highways located within the Sylmar Community Plan Area include: The I-210 freeway from the 118 interchange to the 5 North interchange and the I-5 North from the I-210 interchange to the SR-14 split.	Scenic Highways	M7.1	DCP
P92	Transit Connections to key Areas. Coordinate with LADOT to initiate a study for possible DASH routes within the community to supplement the Metro bus services in allowing mobility between destinations in Sylmar, including but not limited to: the Sylmar/San Fernando Metrolink Station, Los Angeles Mission College, Olive View-UCLA Medical Center, parks and open spaces, and commercial shopping centers.	Public Transit	M8.1	LADOT, METRO
P93	Coordination. Coordinate with local and regional public transit operations to provide expanded public transit options in corridors with high travel demand, as funding permits.	Public Transit	M8.1	LADOT, METRO

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P94	Metro Buses. Extend the hours of service operations and reduce headways of Metro buses to rail stations, as funding permits.	Public Transit	M8.1	LADOT, METRO
P95	Development at Transit Nodes. The Plan designates lands for higher residential densities and commercial intensities within transit oriented development (TOD) areas.	Public Transit	M8.2	DCP
P96	CPIO. The Plan adopts a Transit Oriented Development (TOD) subdistrict in the Community Plan Implementation Overlay Zone (CPIO) which promotes connectivity to and from the Sylmar/San Fernando Metrolink Station.	Public Transit	M8.2	DCP
P97	Encourage TOD Development. The Plan facilitates new infill development projects to locate near major bus centers and transit stations through contextual increases in height and development intensity within major corridors and intersection nodes within the TOD area of the CPIO.	Public Transit	M8.2	DCP
P98	Private Transit. Encourage large development projects to initiate a shuttle bus program to serve as an alternative transit options to the residents of the community.	Public Transit	M8.3	DCP, LADOT
P99	Transit Priority Streets. In addition to the adopted General Plan Mobility Plan 2035 Transit Priority Streets, the Sylmar Community Plan identifies recommended Transit Priority Streets, as shown on Figure 4.2.	Public Transit	M9.1	DCP
P100	Transit Access and Amenities. Provide well-designed transit amenities such as shelters, transit information kiosks, advanced fare collection systems, lighting, improved sidewalks, street crossing, and benches near all bus stops on arterial streets, as funding permits.	Public Transit	M9.3	DCP, LADOT, METRO, PW
P101	Street Enhancements for Buses. Implement traffic signals control systems that optimize flow throughout a network and provide priorities for high capacity bus systems.	Public Transit	M9.4	LADOT, METRO
P102	Express Bus Focus. Coordinate with public agencies to integrate regional and local transit serving the Sylmar Community Plan area.	Public Transit	M9.5	LADOT, METRO
P103	High Speed Rail. Conduct studies to determine the appropriate uses and amenities necessary to increase ridership, while balancing the needs of the community, if a high-speed rail station or line is planned for Sylmar.	Public Transit	M9.6	DCP
P104	Priorities for Capacity Enhancements. Implement peak-hour parking restrictions and striping for additional lanes where feasible and warranted.	Motorized Vehicles	M10.1	LADOT
P105	Street Standards. Improve streets to their designated standard specifications when development project require dedication and improvement.	Motorized Vehicles	M10.1	LADOT, PW
P106	Turning Lanes. Provide right and left turn lanes on arterial streets where warranted.	Motorized Vehicles	M10.1	LADOT

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P107	Motorized Vehicle Priority Streets. The Plan identifies motorized vehicle priority streets, as shown on Figure 4-2 Priority Streets. These streets include: Foothill Boulevard (entire length).	Motorized Vehicles	M10.2	DCP
P108	Coordination. Coordinate with the Bureau of Street Services to improve roadway conditions and maintenance repair.	Motorized Vehicles	M10.2	PW
P109	Access Management. Require that new development projects be designed to minimize disturbance to existing vehicle circulation with proper ingress and egress to parking and drop off areas.	Motorized Vehicles	M10.4	DCP, LADOT
P110	Drop Off Areas. Require adequate drop-off areas for schools, day care, health care, and other uses with intensive passenger drop-off demand.	Motorized Vehicles	M10.4	DCP, LADOT
P111	Alley Access. Encourage parking access from alleys only, where alley access is available in commercial districts.	Motorized Vehicles	M10.5	DCP, LADOT
P112	Hillside Streets. Implement clear and consistent signage to delineate no street parking on Red Flag Days for hillside streets in designated fire severity zones.	Motorized Vehicles	M10.6	LADOT
P113	Emergency Access and Evacuation Routes. Require all new development to address emergency access and evacuation routes during site planning.	Motorized Vehicles	M10.6	DCP, LAFD, LADOT
P114	Coordinated Evacuation Routes. Coordinate with emergency service providers to ensure continued service operations and levels of service.	Motorized Vehicles	M10.7	LAFD, LAPD, LADOT
P115	Traffic Calming. Implement neighborhood preferential parking where appropriate such as low density residential neighborhoods located adjacent to TOD areas and associated traffic impacts warranted.	Motorized Vehicles	M11.1	LADOT
P116	Traffic Mitigations for Development. Enforce overnight parking regulations for commercial, recreational and other non-conforming vehicles in residential neighborhoods.	Motorized Vehicles	M11.2	LADOT
P117	Traffic Studies. Require traffic studies for major developments that identify neighborhood impacts and corresponding mitigations.	Motorized Vehicles	M11.2	DCP, LADOT
P118	Special Event Coordination. Prepare and implement special traffic management plans to mitigate the impact of street closures associated with special events.	Motorized Vehicles	M11.3	LADOT, PW, LAPD, LAFD
P119	Regional Coordination. Collaborate with other public agencies to improve shuttle services to employment, recreation, and entertainment destinations.	Motorized Vehicles	M12.1	LADOT, METRO
P120	Alternatives to the Automobile. Coordinate with other agencies that conduct demonstration programs for Local Use Vehicles (LUV) and identify areas where these vehicles can be used to reduce greenhouse gas emissions, air pollution and gasoline consumption. These programs utilize 100% electric, zero emission LUV that are small, short range, and low speed for taking "local" trips around the community.	Motorized Vehicles	M12.3	LADOT, METRO

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P121	Electric Vehicles. Coordinate the LADWP to promote the utilization of electric vehicles and other forms of electric transportation as a means of improving both air quality and economic development.	Motorized Vehicles	M12.3	LADWP, LADOT
P122	TDM Plans. Encourage the formation of sustainable Transportation Management Associations to implement TDM Plans.	Motorized Vehicles	M12.4, M12.5	DCP, LADOT
P123	TDM Ordinance. Requirement to develop and submit TDM Plans are included in the Citywide TDM Ordinance.	Motorized Vehicles	M12.4	DCP, LADOT
P124	Transportation Management Associations. Apply for grants to support and expand ride share activities coordinated by TMAs.	Motorized Vehicles	M12.5	LADOT, Metro
P125	Industrial Center Siting. Coordinate with Caltrans to improve direct freeway access to distribution centers and other industrial districts.	Goods Movement	M13.1	LADOT, CalTrans
P126	Truck and Freight Movement. The Mobility Plan 2035 of the City's General Plan identifies truck routes that are intended to minimize noise and vibration impacts on sensitive land uses and mark with appropriate signage.	Goods Movement	M13.4	DCP, LADOT
P127	On-site Loading. Collaborate with business owners/operators in industrial districts to identify deficiencies in access, loading and parking on existing streets and develop a strategy to address the deficiencies.	Goods Movement	M13.5	DCP, LADOT
P128	Site Plan Review and Loading Areas. Require Site Plan Review for mixed use and commercial developments to ensure adequate loading areas.	Goods Movement	M13.5	DCP
P129	Parking Management Districts. This policy is implemented through provisions of the LAMC which allow businesses and property owners to meet parking requirements off-site at centralized garages or shared facilities.	Parking Management	M14.1	DCP, LADOT, LADBS
P130	Parking. Encourage the business community and property owners to provide public automobile and bicycle parking that is close to destinations for customer needs and use outlying parking in non-residential areas.	Parking Management	M14.1	DCP, LADOT
P131	Shared Parking. Require developers of mixed use projects to incorporate shared use parking concepts into the project design.	Parking Management	M14.1	DCP, LADOT
P132	Parking Districts. Preferential Parking Districts are implemented through the LADOT and consideration of such districts in areas where excessive abandonment and overnight intrusion of outside vehicles can be verified is strongly encouraged.	Parking Management	M14.1	DCP, LADOT
P133	Performance Based Parking Supply. Conduct a parking needs assessment to identify parking supply in selected districts. Where assessments indicate excess potential, implement a parking program similar to [the Eagle Rock Community Pilot Project] that encourages use of "pooled" parking resources to satisfy parking requirements for change of use projects.	Parking Management	M14.2	DCP, LADOT

Table 6.2 Implementation Programs -
Chapter 4 - Mobility

Program Number	Program Description	Policy/ Section	Policy Reference Number	Responsible or Coordinating Agency
P134	Conversion of Surface Lots to Structures. Coordinate with other public agencies to develop public parking structures in Transit Oriented Districts, Mixed Use and Commercial Corridors.	Parking Management	M14.3	DCP, LADOT
P135	Convenient Parking. Encourage the business community to consider offering monthly parking leases to Metro commuters and/or dedicated parking spaces for shared public vehicles and bicycles.	Parking Management	M14.4	DCP, LADOT, Metro
P136	Reduced Parking near Transit Centers. The LAMC allows a 10% parking incentive for projects located within 500 feet of a mass transit station portal.	Parking Management	M15.1	DCP, LADBS
P137	Park and Ride. Identify the need and potential locations for "park and ride" or other shared facilities within the transit oriented development CPIO sub-districts of the Sylmar Community Plan area toward alleviating the need for individual businesses located within these TOD areas to address required parking on-site.	Parking Management	M15.1	DCP, LADOT, METRO
P138	Car and Bike Sharing Programs. Encourage new projects to incorporate car share and bike share parking programs.	Parking Management	M15.1	DCP, LADOT
P139	Trip Reduction Credits. Develop alternative traffic mitigation programs such as trip reduction credits for integrating bike share and car share options into new developments near transit centers.	Parking Management	M15.1	DCP, LADOT
P140	Public Amenities. Require developments located near public transit to implement amenities such as bicycle lockers and bicycle repair.	Parking Management	M15.1	DCP, LADOT, Metro
P141	Reduced Parking Requirements. The Plan establishes reduced parking requirements tailored to individual transit oriented development (TOD) subareas.	Parking Management	M15.1	DCP
P142	Park Once Strategy. Support the installation of automated parking guidance systems where appropriate in City-owned facilities and encourage their use in privately operated facilities.	Parking Management	M15.2	DCP, LADOT
P143	Priority Parking for Alternative Fuel Vehicles. Require developers to provide priority parking spaces for alternative fuel vehicles for new major development projects.	Parking Management	M15.3	DCP, LADOT
P144	Connection for Electric Vehicles. Work with the LADWP to develop standards for power service to new development garages and parking areas.	Parking Management	M15.4	LADWP, LADOT

Table 6.3 Implementation Programs -
Chapter 5 - Community Facilities, Infrastructure, and Open Space

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P145	Level of Service. Consult with the Los Angeles Police Department (LAPD) as part of the review of new development projects and proposed land use changes to determine law enforcement needs and services.	Police	CF1.1-1.2	DCP, LAPD
P146	Community Watch. Encourage the development and operation of community-based crime prevention measures, such as, but not limited to, neighborhood watches, citizen patrol committees, and organized clean-up days through regular interaction and coordination with local law enforcement and communication with the Neighborhood Council.	Police	CF1.1-1.2	DCP, LAPD
P147	BID. Encourage Business Improvement Districts to supplement patrol services with private security services through training and coordination programs administered by the LAPD.	Police	CF1.1-1.4	LAPD, BID
P148	Project Review. Incorporate LAPD's "Design Out Crime" guidelines in discretionary land use and approvals. (http://www.lapdonline.org/crime_prevention/content_basic_view/8852)	Police	CF1.2	DCP, LAPD
P149	Design for Security. Establish reasonable defensible space design requirements that will help ensure maximum visibility and security for entrances, pathways, and corridors, as well as open space, parking lots, and structures. Ensure that landscaping around buildings does not impede visibility or provide hidden places that could foster criminal activity.	Police	CF1.2	DCP
P150	Fire and Emergency Services. Consult with the Los Angeles Fire Department's Planning and Fire Protection Section and the Public Safety Bureau to review and implement standards for the location and expansion of fire facilities and emergency medical services.	Fire and Emergency Services	CF2.1	DCP, LAFD
P151	Coordination. Coordinate with the Los Angeles Fire Department during review of discretionary projects affecting land use changes to determine the impacts related to fire protection and emergency medical services.	Fire and Emergency Services	CF2.2	DCP, LAFD
P152	Project Review. Through discretionary review, the decision maker should include a finding regarding the impact on fire service infrastructure of proposed projects or land use changes.	Fire and Emergency Services	CF2.2	DCP, LAFD
P153	Emergency Preparedness Coordination. Require new developments to address emergency access and egress in site planning.	Fire and Emergency Services	CF2.3	DCP, LAFD
P154	Coordination. Coordinate with the Library Department to identify areas as preferred locations for new branch library facilities within the community, as funding becomes available.	Libraries	CF3.1	LAPL
P155	Land Use. Existing library sites remain designated in the Public Facilities land use category and Public Facility (PF) zone. This designation provides protection to retain the existing uses on site which allows for greater certainty for needed City approvals when rehabilitating or expanding structures on site.	Libraries	CF3.1	DCP

Table 6.3 Implementation Programs -
Chapter 5 - Community Facilities, Infrastructure, and Open Space

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P156	Flexibility of Siting. Encourage new development to site library services in commercial centers, community centers, office buildings, pedestrian oriented areas that is available and accessible to the public.	Libraries	CF3.2	DCP, LAPL
P157	Joint Use. Encourage the Los Angeles Library Department, Los Angeles Unified School District, and other applicable agencies, to maximize the accessibility of public facilities to neighborhood organizations and community groups for civic use.	Libraries	CF3.3	LAPL, LAUSD
P158	Existing Facilities. The Plan identifies recently developed school sites and corrects any zoning inconsistency through a zone change to Public Facility (PF). This new designation provides more protection to retain the existing uses on site which allows for greater certainty for needed City approvals when rehabilitating or expanding structures on site.	Schools	CF4.1	DCP
P159	Facility Location. The Plan Map identifies existing and recently developed public facility sites and corrects zoning inconsistencies through zone changes to match the land use designation. The land use designation provides protection to retain and expand school uses.	Schools	CF4.3	DCP
P160	Streetscape. Encourage the location of new schools in areas where established walkways, bicycle paths, or greenways link the proposed school with the surrounding land uses.	Schools	CF4.3	DCP, LAUSD
P161	Compatible School Sites. Design fencing to enable community connections and discourage chain-link fencing around school properties. Develop design criteria to ensure that new schools are sited and designed to complement the existing identity of stable single and multi-family residential neighborhoods.	Schools	CF4.4	DCP, LAUSD
P162	Joint Use of Facilities with LAUSD. Encourage the Los Angeles Unified School District, Los Angeles Department of Recreation and Parks, and other applicable agencies, to maximize the accessibility of public facilities to neighborhood organizations and community groups for recreation and civic use.	Schools	CF4.5	LARAP, LAUSD
P163	Joint Use Program with RAP. Continue the long standing Joint Use Program established throughout the LAUSD and RAP.	Schools	CF4.5	LAUSD, RAP
P164	Joint Use/Innovation Fund. This policy is implemented through LAUSD's Joint Use/Innovation Fund.	Schools	CF4.5	LAUSD
P165	Parkland Preservation. The Plan Map designates lands for open space uses, including passive and active recreational parks to be preserved and enhanced.	Parks	CF5.1	DCP
P166	Open Space Conservation. Maintain all Open Space designations within the Sylmar CPA and designate parkland as Open Space as it is acquired by the Department of Recreation and Parks.	Parks	CF5.1	DCP
P167	Site Enhancements. This policy is implemented through carrying out the "needs assessment" strategies of the Department of Recreation and Parks in developing a Citywide Recreation and Parks Master Plan.	Parks	CF5.2	RAP

Table 6.3 Implementation Programs -
Chapter 5 - Community Facilities, Infrastructure, and Open Space

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P168	Surplus Properties. Coordinate with public agencies to review, evaluate, and acquire surplus property and vacant parcels for publicly owned open space, parks and recreational facilities.	Parks	CF5.3	DCP, GSD
P169	Coordination. Coordinate with other city department and public agencies to expand open space and recreational opportunities within the community.	Parks	CF5.3	DCP
P170	Vacant Land. Coordinate with public agencies to review, evaluate, and acquire surplus property and vacant parcels for publicly owned open space, parks and recreational facilities.	Parks	CF5.4	DCP, GSD
P171	Location. Maintain flexibility about the park size and facilities to be included in order to take advantage of new opportunities as they arise.	Parks	CF5.7	LARAP
P172	Joint Use. Encourage LAUSD to develop school-specific agreement to enable members of the community to jointly use the facilities for recreational activities.	Parks	CF5.9	LAUSD
P173	Joint Use Program with RAP. Continue the long standing Joint Use Program established through the LAUSD and RAP.	Parks	CF5.9	LAUSD, RAP
P174	Joint Use/Innovation Fund. This policy is implemented through LAUSD's Joint Use/Innovation Fund.	Parks	CF5.9	LAUSD
P175	Public Transit. Coordinate with local and regional public transit operators to provide expanded public transit options in corridors with high travel demand, as funding permits.	Parks	CF5.10	LADOT, METRO
P176	Private Investment. Seek private sector and Metro funding for shuttle routes connecting Sylmar neighborhoods to the Sylmar/San Fernando Metrolink Station, the Los Angeles Mission Community College, Olive View Medical Center, Industrial areas/jobs, and parks and open space areas within the Community Plan Area.	Parks	CF5.10	METRO, LADOT
P177	OS Land Use. The Plan Map designates lands for open space uses, including passive and active recreational parks to be preserved and enhanced.	Open Space	CF6.1-6.2	DCP
P178	Conservation. Maintain all Open Space designations within the Sylmar CPA and designate parkland as Open Space as it is acquired by the Department of Recreation and Parks.	Open Space	CF6.1-6.2	DCP
P179	Protection. The Plan brings zones and land uses into consistency with the use of subject and adjacent properties.	Open Space	CF6.2	DCP
P180	Trail Linkages. Implement the Sylmar Trail Network, as shown on Figure 4-5 of the Sylmar Community Plan through conditioning discretionary approval to ensure connection to open space areas and recreational trails when development is proposed within proximity of a mapped trail.	Open Space	CF6.8	DCP, LADBS, PW

Table 6.3 Implementation Programs -
Chapter 5 - Community Facilities, Infrastructure, and Open Space

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P181	Pacoima Wash and Greenway Network. Coordinate with non-profits and other appropriate agencies to preserve and enhance the Pacoima Wash by incorporating greenway improvements and recreational amenities.	Open Space	CF7.2	DCP, LA County Public Works, Army Corps (US Govt)
P182	Pacoima Wash. Protect, enhance, and restore the Pacoima Wash as a natural area and protect it from encroachment by coordinating the implementation of the Pacoima Wash Vision Plan and the Pacoima Wash Greenway Master Plan in conjunction with the Pacoima Beautiful, the City of San Fernando, and other partnering agencies.	Open Space	CF7.2	DCP, PW
P183	Urban Forest. Collaborate with tree planting groups, non-profits, and local schools to plant trees and increase the urban forest throughout the community.	Urban Forest	CF8.1	DCP, PW
P184	Shade Streets. Develop measures to reduce heat gain from pavement and other hardscaping for new development.	Urban Forest	CF8.5	DCP, Urban Forestry, BSS
P185	Local Water Resources Optimization. Ensure compliance with the City's Water Supply Action Plan, as well as the Securing LA's Water Supply Plan and LADWP's Urban Water Management Plan.	Water	CF9.1	LADWP
P186	Conserve Water. Continue to implement existing water conservation measures, including enforcement of the existing Water Efficiency Requirements ordinance. These measures include the use of water-efficient landscaping and irrigation, storm water capture, efficient appliances, the use of recycled water for irrigation, and minimizing the amount of non-roof impervious surfaces around buildings.	Water	CF9.2	LADWP
P187	Wastewater Minimization. Ensure compliance with the Bureau of Sanitation's Wastewater Integrated Resource Plan (IRP).	Wastewater	CF10.1	DWP, PW
P188	On-site Wastewaters Output. The Los Angeles Bureau of Sanitation Biosolids Environmental Management Program recovers residues from its wastewater treatment plants. The Hyperion Treatment Plant is responsible for managing the residues that are produced from the processing of wastewater.	Wastewater	CF10.2	LADWP, PW
P189	Recycled Water. The East Valley Water Recycling Project produces high quality, extensively treated recycled water for irrigation and other non-drinking uses for industrial and commercial businesses in the San Fernando Valley.	Wastewater	CF10.3	PW, LADWP
P190	Waste Reduction. Ensure compliance with ClimateLA which sets the goal of reducing or recycling 70% of waste by 2015.	Solid Waste	CF11.1	PW
P191	On-Site Recycling. Support the efforts of the Bureau of Sanitation's Multiple-Family Residential Recycling Program for residential buildings of five units or more.	Solid Waste	CF11.2	PW
P192	Recycled Materials. Ensure compliance with the Citywide Construction and Demolition (C&D) Waste Recycling Ordinance. (Bureau of Sanitation)	Solid Waste	CF11.3	PW, LADBS

Table 6.3 Implementation Programs -
Chapter 5 - Community Facilities, Infrastructure, and Open Space

Program Number	Program Description	Policy/Section	Policy Reference Number	Responsible or Coordinating Agency
P193	Interdepartmental Coordination. This policy is addressed through implementation of Bureau of Sanitation's Solid Waste Integrated Resource Plan (IRP).	Solid Waste	CF11.4	PW, DCP
P194	Watershed Revitalization. Ensure compliance with the Water Quality Compliance Master Plan for Urban Runoff (WQCMPUR) as administered through the Watershed Protection Division's Stormwater Program.	Stormwater	CF12.1	PW
P195	Groundwater Infiltration. Require the incorporation of bio-retention practices and use of permeable materials for the paving of sidewalks, driveways, and parking areas, where feasible.	Stormwater	CF12.3	PW
P196	Management Practices. Incorporate Best Management Practices in the design of new development to recapture and reclaim stormwater, including but not limited to: permeable pavement, french drains, curb depressions, and catch basins.	Stormwater	CF12.3	PW, DCP
P197	Permeable Surfaces. Reduce areas of impermeable surfaces in order to create areas that detain stormwater runoff and allow for groundwater infiltration to recharge the natural underground water table.	Stormwater	CF12.3	PW, DCP
P198	Recycled Water. Recycled water processed at the East Valley Water Recycling Project is diverted from the Sepulveda Basin to the northeast San Fernando Valley for recharge of the underground water table.	Stormwater	CF12.3	LADWP, PW
P199	Utility Easements. The Plan designates land for various public facility uses, as shown on the Land Use Map.	Energy	CF13.1	DCP
P200	Electrical Facilities. Consult with the LADWP during discretionary review to ensure that energy demands of proposed developments will be met.	Energy	CF13.2	LADWP, DCP
P201	Underground Utilities. The Plan encourages all new development to locate utility equipment to the rear of the site and/or be screened by abundant landscaping in order to enhance the streetscape environment.	Energy	CF13.4	LADWP, DCP
P202	Utility Infrastructure Loan Program. Expand the use of LADWP Utility Infrastructure Loan Program, which assists qualified businesses with short-term, low cost financing options for undergrounding utilities.	Energy	CF13.4	LADWP
P203	Energy Management. Coordinate with the Bureau of Street Lighting to implement the LED Street Lighting Efficiency Program to replace existing street lighting with new LED solid-state fixtures	Street Lighting	CF15.1	LADWP, PW
P204	Lighting Integration. Refer to the Design Guidelines for policies and programs regarding special pedestrian lighting in commercial centers and neighborhood districts.	Street Lighting	CF15.2	DCP, PW

Sources of Funding

The development, maintenance, and operation of parks, public facilities and improvements (i.e., in the public right-of-way), and the provision of city services require financial resources that are derived from various sources. Programming of city capital projects and their funding over time is outlined in the city’s Capital Improvement Program. Although the Community Plan does not mandate specific capital improvements, the policies serve as a guide for other city agencies to identify and budget for potential future capital projects. Typical revenue sources used to fund these projects include property tax revenue, sales tax revenue, user fees, Quimby Act (Park) dedications, business improvement districts, special assessment districts, municipal bonds, and county, state, and federal funding.

It is important to note that program implementation is contingent on the availability of adequate funding, which is likely to change over time due to economic conditions, the priorities of federal, state and regional governments and funding agencies, and other conditions. Some typical funding sources for public services and infrastructure are identified in Table 6-4.

Potential funding sources which are considered to be the most likely funding sources include a development impact fee, tax allocation bonds, Metro funds, business improvement districts and various public revenue funds. Each one of these most likely funding sources are detailed below.

TABLE 6-4
Potential Funding Source for Policies and Programs

Fees and Exactions	Taxes
Quimby Fees Development Impact Fees Permit and Application Fees Regulatory Fees Property Assessments <ul style="list-style-type: none"> • Benefit Assessment District • Business Improvement District • Vehicle Parking District 	Special taxes such as Mello-Roos Community Facilities Districts Taxes for mobility improvements which are allocated by Metro’s Call for Projects: <ul style="list-style-type: none"> • Proposition A sales tax • Proposition C sales tax • State retail sales tax • State motor vehicle tax • Federal gas tax
Bonds	Public Revenue Funds
Tax allocation bonds issued by the Community Redevelopment Agency Proposition 1B state bonds.	City’s General Fund LADOT Operating Budget City Capital Improvement Program Caltrans Capital Improvement Fund Special Parking Revenue Fund

